

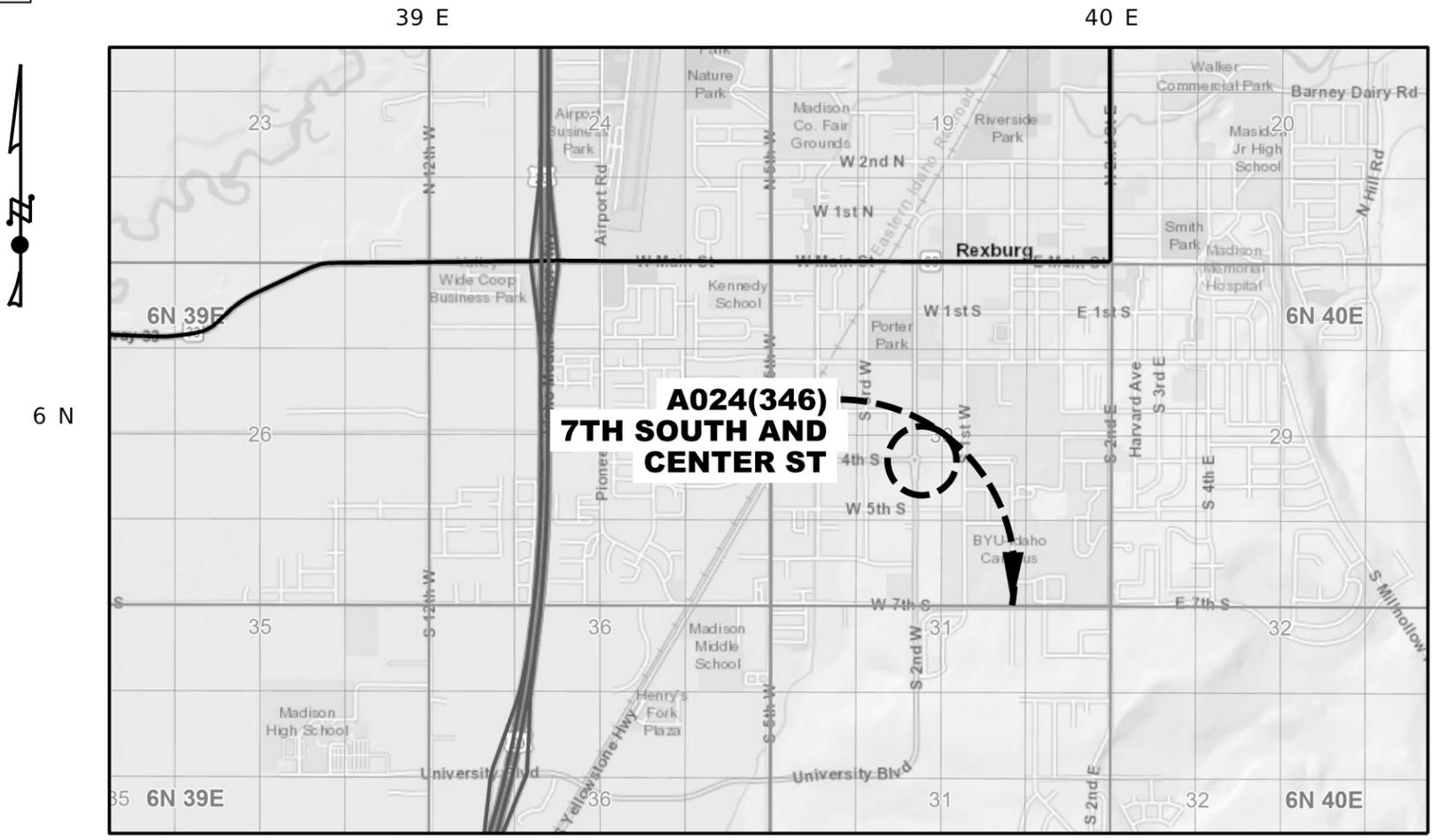
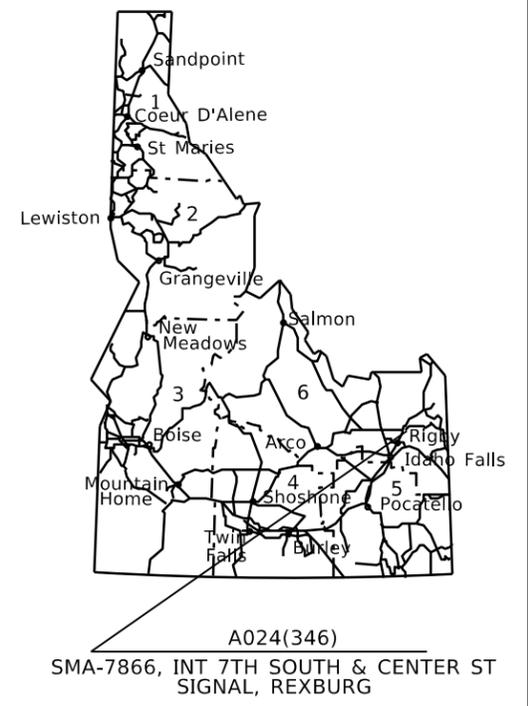
INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	PROJECT CLEARANCE SUMMARY
3	TYPICAL SECTION
4	ROADWAY SUMMARY SHEET
5	SITE PREPARATION PLAN
6-7	PLAN SHEET
8-11	CURB DETAIL SHEET
12-14	MATERIALS QUANTITY SHEET
15	SIGNAL INTERSECTION PLAN
16	CONDUIT DIAGRAM
17	CABINET DETAIL
18	SIGNAL FIELD WIRE DIAGRAM
19	SIGNAL POLE MOUNTING DETAIL
20-21	TRAFFIC SIGNAL DETAILS
22-24	DETAIL SHEETS
25	STRIPING PLAN SHEET
26-34	TEMPORARY TRAFFIC CONTROL
1-21	ITD STANDARD DRAWINGS

IDAHO TRANSPORTATION DEPARTMENT

PLAN AND PROFILE OF PROPOSED SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

FEDERAL AID PROJECT NO. A024(346) KEY NO. 24346 MADISON COUNTY

SEPTEMBER 2025



DESIGN DESIGNATION

ADT 2027	10930
ADT 2047	11360
DHV 2027	1530
DHV 2047	1590
D	60/40
V	35 MPH
TRUCKS:	
ADT 2027	110
ADT 2047	110
DHV 2027	10
DHV 2047	10

REVISIONS			
NO.	DATE	BY	DESCRIPTION

THE DIMENSIONS SHOWN ON THE PLANS SHALL BE ATTAINED WITHIN LIMITS OF PRECISION THAT GOOD CONSTRUCTION PRACTICES WILL PERMIT

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME 24346_title.DGN
DRAWING DATE: MAY 2024

**IDAHO
TRANSPORTATION DEPARTMENT**

YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

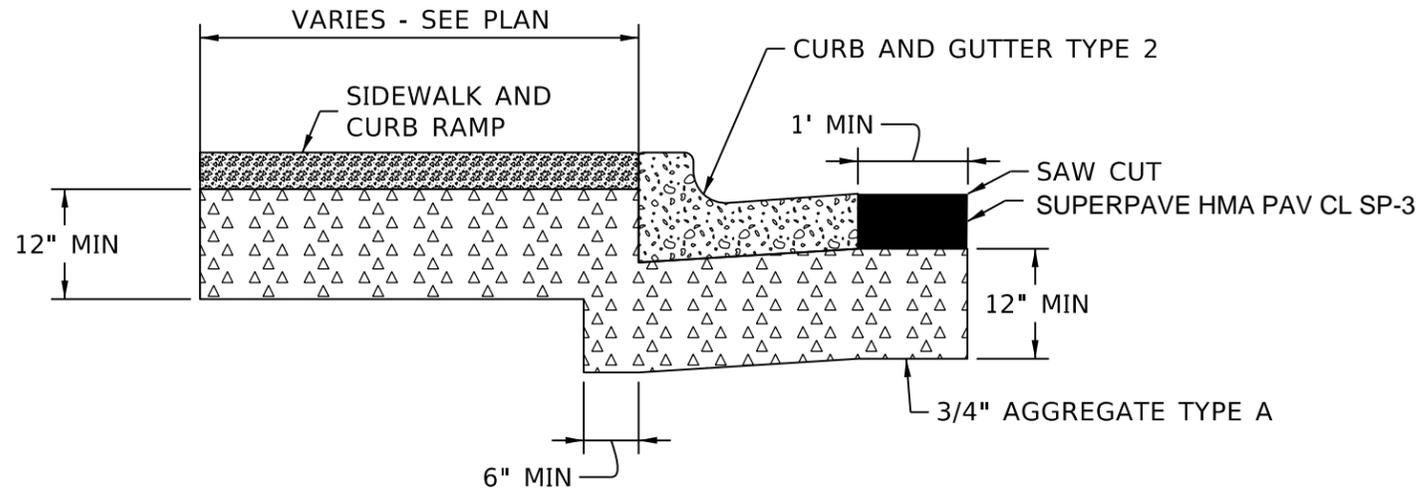
PROJECT NO.
A024(346)

TITLE SHEET
SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH
COUNTY MADISON
KEY NUMBER 24346
SHEET 1 OF 34

Saile Wae
Approved for Advertising
11/10/2025
Date Approved

TYPICAL SECTION
CURB, GUTTER, AND SIDEWALK



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME 24346_typ.dgn
DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

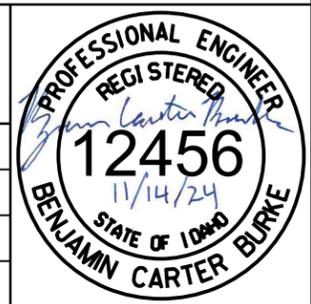
Horrocks.



PROJECT NO.	A024(346)
-------------	-----------

TYPICAL SECTION	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
-----------------	---

ENGLISH
COUNTY MADISON
KEY NUMBER 24346
SHEET 3 OF 34



7TH SOUTH AND CENTER ST

ITEM NO.	ITEM	UNIT	TOTAL
203-006A	REMOVAL OF SIGN	EACH	4
203-060A	REMOVAL OF CONCRETE SIDEWALK	SY	133
203-070A	REMOVAL OF CURB AND GUTTER	FT	147
203-122A	REMOVAL OF MISCELLANEOUS ITEMS (ILLUMINATION)	EACH	3
203-130B	REMOVAL OF PAVEMENT MARKINGS	SF	809
212-095A	INLET PROTECTION	EACH	2
303-021A	3/4" AGGREGATE TYPE A FOR BASE	TON	126
405-435A	SUPERPAVE HMA PAVEMENT INCLUDING ASPHALT & ADDITIVES CLASS SP-3	TON	3.5
614-015A	SIDEWALK	SY	72
614-025A	CURB RAMP	SY	62
615-492A	CURB AND GUTTER TYPE 2	FT	143
616-090A	SPECIAL INSTALL SIGN	EACH	4
619-010A	ILLUMINATION TYPE 2	LS	1
626-100A	MISCELLANEOUS TEMPORARY TRAFFIC CONTROL ITEMS	CA	5000
630-010A	TRANSVERSE, WORD, SYMBOL, AND ARROW PAVEMENT MARKINGS - PREFORMED THERMOPLASTIC	SF	843
630-025A	LONGITUDINAL PAVEMENT MARKING - WATERBORNE	FT	5,639
656-010A	SIGNAL HOUSING	EACH	10
656-020C	INTERSECTION DETECTION SYSTEM-VIDEO	EACH	1
656-025A	ELECTRIC SERVICE PEDESTAL TYPE 1	EACH	1
656-030A	INTERSECTION SIGNAL STRUCTURE	EACH	1
656-035A	INTERSECTION PEDESTRAIN SYSTEM	EACH	1
656-040A	SIGNAL CABINET	EACH	1
656-045A	SIGNAL CONTROLLER	EACH	1
656-050A	PEDESTRIAN & TRAFFIC CONTROL SIGNAL HEAD FRANGIBLE POLES	EACH	7
656-065A	COMBINED CABINET & SERVICE PEDESTAL FOUNDATION	EACH	1
656-070A	ADDITIONAL ELECTRICAL ITEMS	LS	1
675-005A	SURVEY	LS	1
675-010A	DIRECTED SURVEYING	CA	5,000
677-005A	RECORD DRAWINGS	LS	1
S900-50A	CONTINGENCY AMOUNT MISCELLANEOUS WORK	CA	5,000
S900-50B	CONTINGENCY AMOUNT DIRECTED ROCK EXCAVATION	CA	25,000
S904-05A	SP-TEMPORARY TRAFFIC CONTROL	LS	1
Z629-05A	MOBILIZATION	LS	1

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY CADD FILE NAME 24346_rsum_001.dgn DRAWING DATE: November 14, 2024
DESIGN CHECKED B. BURKE	
DETAILED R. TELFORD	
DRAWING CHECKED B. BURKE	

IDAHO
TRANSPORTATION
DEPARTMENT
YOUR Safety--YOUR Mobility--YOUR Economic Opportunity



Horrocks.

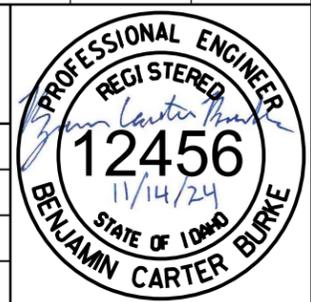
PROJECT NO.

A024(346)

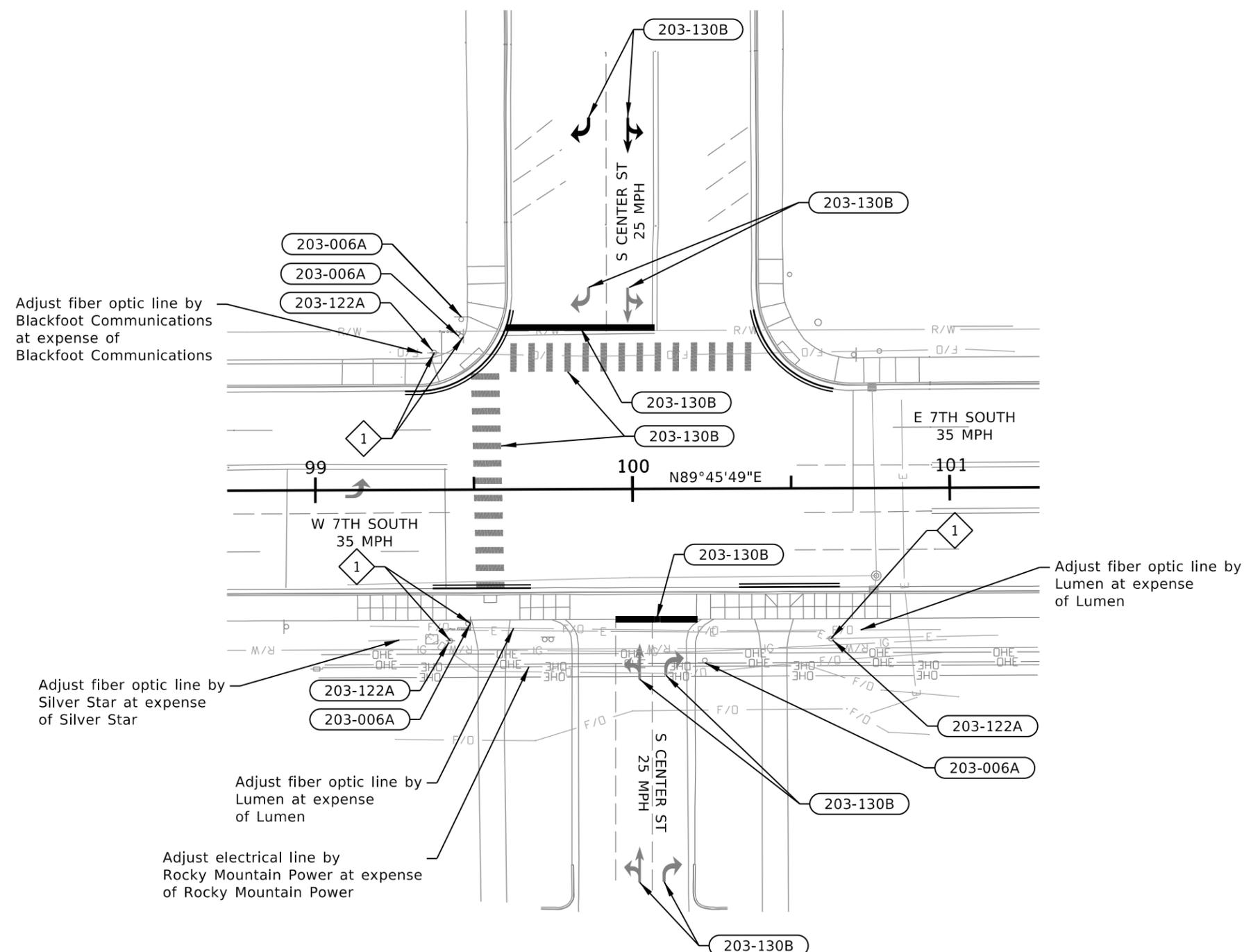
ROADWAY SUMMARY SHEET

SMA-7866, INT 7TH SOUTH
& CENTER ST
SIGNAL, REXBURG

ENGLISH
COUNTY MADISON
KEY NUMBER 24346
SHEET 4 OF 34



T. 6 N., R. 40 E., B.M.



203-006A	Removal of Sign
1 EACH	99+46.2, 48.3' LT
1 EACH	99+46.2, 53.6' LT
1 EACH	99+48.0, 42.1' RT
1 EACH	100+22.6, 54.0' RT
203-122A	Removal of Miscellaneous Items (Illumination)
1 EACH	99+37.7, 43.1' LT
1 EACH	99+42.4, 47.5' RT
1 EACH	100+62.4, 47.3' RT
203-130B	Removal of Pavement Markings
809 SF	

Adjust fiber optic line by Blackfoot Communications at expense of Blackfoot Communications

Adjust fiber optic line by Silver Star at expense of Silver Star

Adjust fiber optic line by Lumen at expense of Lumen

Adjust electrical line by Rocky Mountain Power at expense of Rocky Mountain Power

Adjust fiber optic line by Lumen at expense of Lumen

- CONSTRUCTION NOTES:**
- COORDINATE WITH THE CITY PRIOR TO REMOVING POLE FOUNDATIONS.
 - RETAIN AND PROTECT ALL UTILITIES UNLESS OTHERWISE NOTED.



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_rem_001.dgn
 DRAWING DATE: November 14, 2024

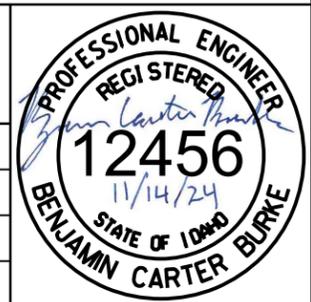
IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

Horrocks.

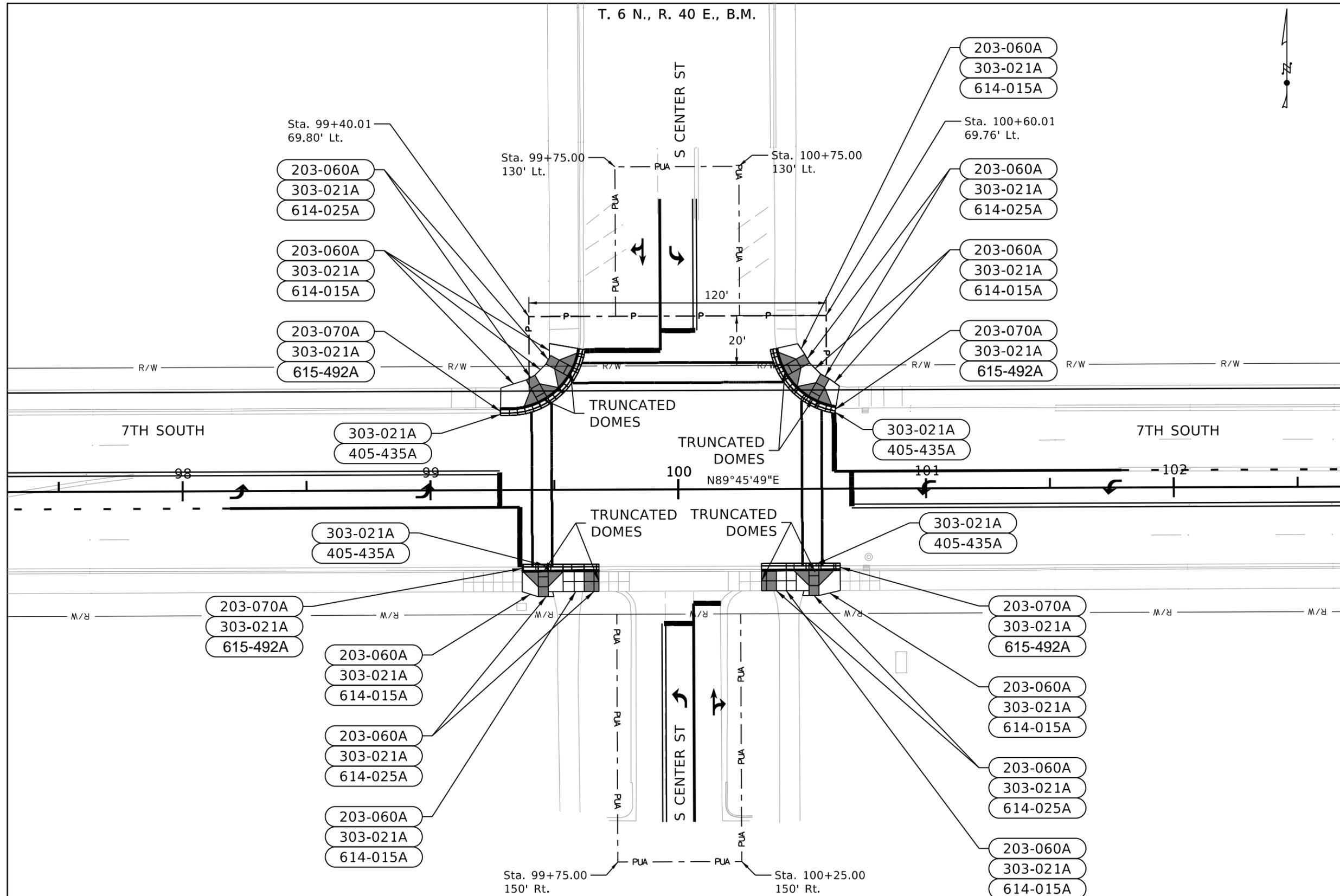
PROJECT NO.
 A024(346)

SITE PREPERATION PLAN
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 5 OF 34



T. 6 N., R. 40 E., B.M.



203-060A	Removal of Concrete Sidewalk
11.7 SY	99+34.3, 38.2' l
8.4 SY	99+43.6, 39.8' l
6.2 SY	99+47.4, 45.6' l
8.4 SY	99+53.3, 49.4' l
5.5 SY	99+53.1, 55.7' l
4.5 SY	99+39.8, 38.6' r
8.5 SY	99+45.4, 36.6' r
11.3 SY	99+55.3, 37.6' r
5.3 SY	99+64.8, 38.0' r
5.3 SY	100+36.5, 37.0' r
10.2 SY	100+45.4, 37.7' r
8.5 SY	100+54.6, 36.6' r
6.6 SY	100+61.4, 38.3' r
4.5 SY	100+46.1, 55.4' l
8.4 SY	100+46.8, 49.4' l
6.2 SY	100+46.8, 49.4' l
8.4 SY	100+52.7, 45.6' l
4.4 SY	100+62.4, 39.0' l

203-070A	Removal of Curb and Gutter
44.2 FT	99+28.4, 33.0 l -
	99+59.7, 56.9 l
31.0 FT	99+36.9, 33.0 r -
	99+67.9, 33.0 r
39.5 FT	100+40.3, 56.7 l -
	100+63.8, 33.3 l
31.8 FT	100+33.5, 33.0 r -
	100+65.2, 33.0 r

Continues on Next Page



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED
B BURKE

DESIGN CHECKED
R TELFORD

DETAILED
B BURKE

DRAWING CHECKED
R TELFORD

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346 plan 001.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

Horrocks.

PROJECT NO.
A024(346)

PLAN SHEET
SMA-7866, INT 7TH SOUTH & CENTER ST
SIGNAL, REXBURG

ENGLISH

COUNTY
MADISON

KEY NUMBER
24346

SHEET 6 OF 34



T. 6 N., R. 40 E., B.M.

PAGE INTENTIONALLY LEFT BLANK

303-021A	3/4" Aggregate Type A For Base
3.5 TON	99+28.4, 30.5' L - 99+62.2, 56.4' L
6.6 TON	99+28.4, 32.0' L - 99+60.7, 56.7' L
8.1 TON	99+34.3, 38.2' L
6.0 TON	99+43.6, 39.8' L
4.2 TON	99+47.4, 45.6' L
6.0 TON	99+53.3, 49.4' L
3.8 TON	99+53.1, 55.7' L
2.3 TON	99+36.9, 30.5' R - 99+67.9, 30.5' R
4.7 TON	99+36.9, 32.0' R - 99+67.9, 32.0' R
3.1 TON	99+39.8, 38.6' R
6.1 TON	99+45.4, 36.6' R
7.9 TON	99+55.3, 37.6' R
3.7 TON	99+64.8, 38.0' R
2.4 TON	100+33.5, 30.6' R - 100+65.2, 30.5' R
4.8 TON	100+33.5, 32.0' R - 100+65.3, 32.0' R
3.7 TON	100+36.5, 37.0' R
7.1 TON	100+45.4, 37.7' R
6.1 TON	100+54.6, 36.6' R
4.5 TON	100+61.4, 38.3' R
2.8 TON	100+37.9, 56.2' L - 100+63.4, 30.9' L
5.4 TON	100+39.4, 56.5' L 100+63.6, 32.3' L
3.1 TON	100+46.1, 55.4' L
6.1 TON	100+46.8, 49.4' L
4.2 TON	100+46.8, 49.4' L
6.1 TON	100+52.7, 45.6' L
3.0 TON	100+62.4, 39.0' L

405-435A	Superpave HMA Pavement Including Asphalt & Additives Class SP-3
1.1 TON	99+28.4, 30.5' L - 99+62.2, 56.4' L
0.7 TON	99+36.9, 30.5' R - 99+67.9, 30.5' R
0.8 TON	100+33.5, 30.6' R - 100+65.2, 30.5' R
0.9 TON	100+37.9, 56.2' L - 100+63.4, 30.9' L

614-015A	Sidewalk
11.7 SY	99+34.3, 38.2' L
6.2 SY	99+47.4, 45.6' L
5.5 SY	99+53.1, 55.7' L
4.5 SY	99+39.8, 38.6' R
11.3 SY	99+55.3, 37.6' R
10.2 SY	100+45.4, 37.7' R
6.6 SY	100+61.4, 38.3' R
4.5 SY	100+46.1, 55.4' L
6.2 SY	100+46.8, 49.4' L
4.4 SY	100+62.4, 39.0' L

614-025A	Curb Ramp
8.4 SY	99+43.6, 39.8' L
8.4 SY	99+53.3, 49.4' L
8.5 SY	99+45.4, 36.6' R
5.3 SY	99+64.8, 38.0' R
5.3 SY	100+36.5, 37.0' R
8.5 SY	100+54.6, 36.6' R
8.4 SY	100+46.8, 49.4' L
8.4 SY	100+52.7, 45.6' L

615-492A	Curb and Gutter
44.2 FT	99+28.4, 32.0' L - 99+60.7, 56.7' L
31.0 FT	99+36.9, 32.0' R - 99+67.9, 32.0' R
31.8 FT	100+33.5, 32.0' R - 100+65.2, 32.0' R
36.1 FT	100+39.4, 56.5' L - 100+63.6, 32.3' L

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED B. BURKE	
DETAILED R. TELFORD	CADD FILE NAME 24346 plan 002.dgn
DRAWING CHECKED B. BURKE	DRAWING DATE: November 14, 2024

**IDAHO
TRANSPORTATION
DEPARTMENT**

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

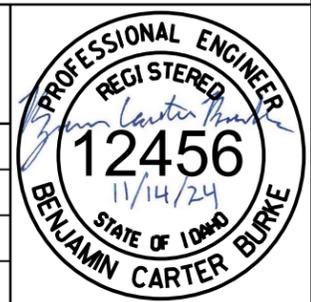


Horrocks.

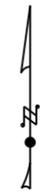
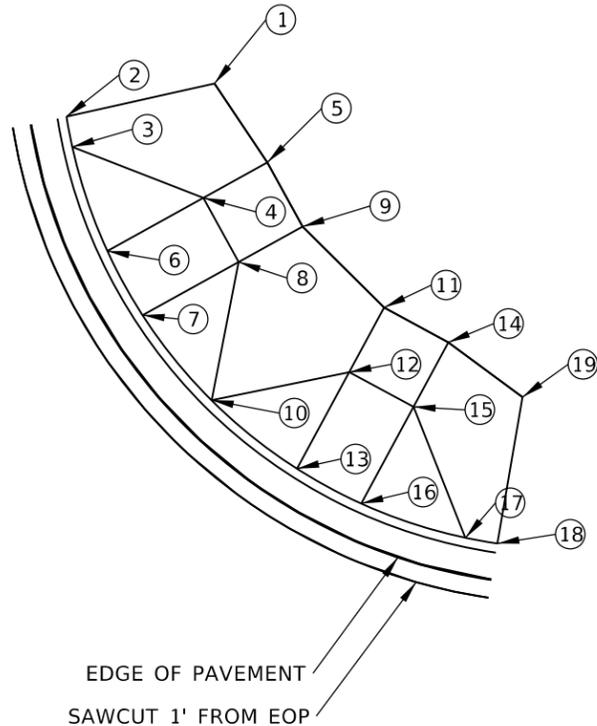
PROJECT NO.	A024(346)
-------------	-----------

PLAN SHEET	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
------------	---

ENGLISH	COUNTY MADISON
	KEY NUMBER 24346
	SHEET 7 OF 34



#	Northing	Easting	Elevation
①	782173.012'	757484.640'	5016.07'
②	782171.214'	757476.540'	5016.33'
③	782169.551'	757476.856'	5016.33'
④	782166.773'	757484.024'	5016.29'
⑤	782168.703'	757487.527'	5016.33'
⑥	782163.877'	757478.769'	5015.99'
⑦	782160.376'	757480.698'	5016.06'
⑧	782163.273'	757485.953'	5016.36'
⑨	782165.203'	757489.456'	5016.40'
⑩	782155.729'	757484.474'	5016.60'
⑪	782160.787'	757493.908'	5016.81'
⑫	782157.268'	757492.006'	5016.77'
⑬	782151.990'	757489.152'	5016.41'
⑭	782158.886'	757497.424'	5016.88'
⑮	782155.367'	757495.522'	5016.84'
⑯	782150.089'	757492.668'	5016.47'
⑰	782148.222'	757498.358'	5016.98'
⑱	782147.906'	757500.111'	5017.02'
⑲	782155.906'	757501.487'	5017.08'



S CENTER ST

NE Curb Ramp

W 7th S 200 100 E 7th S

ORIGINAL STORED
AT: ITD, District 6
5151 South 5th,
Pocatello, ID

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	J. ALLEN
DESIGN CHECKED	B. BURKE
DETAILED	A. HUNTSMAN
DRAWING CHECKED	B. BURKE

SCALES SHOWN
ARE FOR 11" X 17"
PRINTS ONLY

CADD FILE NAME
24346_CR_Det.DGN

DRAWING DATE:
November 14, 2024

**IDAHO
TRANSPORTATION
DEPARTMENT**



Horrocks.

PROJECT NO.

A024(346)

CURB DETAIL SHEET

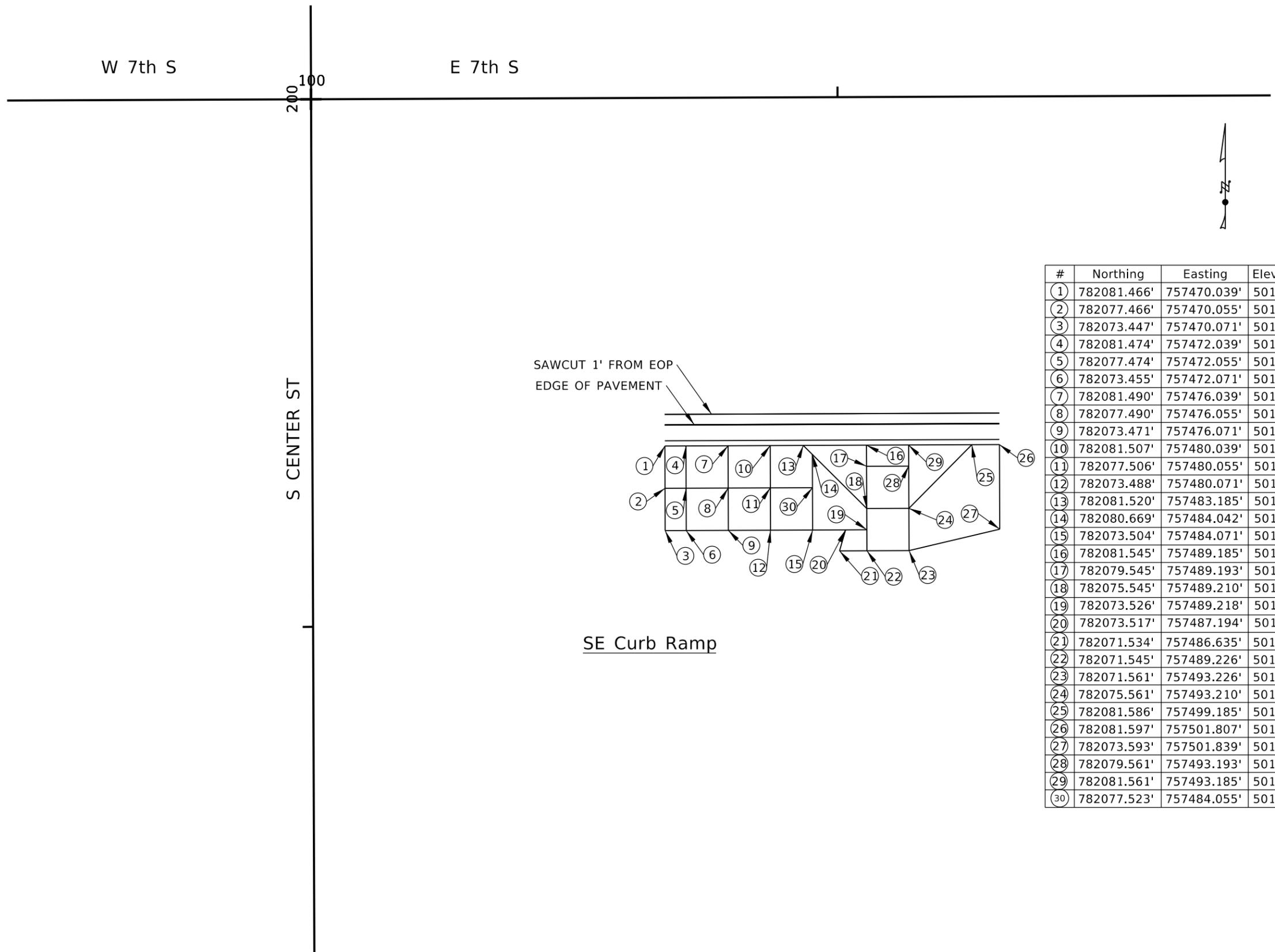
**SMA-7866, INT 7TH SOUTH
& CENTER ST
SIGNAL, REXBURG**

ENGLISH

COUNTY **MADISON**

KEY NUMBER **24346**

SHEET **8** OF **34**



#	Northing	Easting	Elevation
①	782081.466'	757470.039'	5017.29'
②	782077.466'	757470.055'	5017.35'
③	782073.447'	757470.071'	5017.40'
④	782081.474'	757472.039'	5017.34'
⑤	782077.474'	757472.055'	5017.40'
⑥	782073.455'	757472.071'	5017.46'
⑦	782081.490'	757476.039'	5017.45'
⑧	782077.490'	757476.055'	5017.51'
⑨	782073.471'	757476.071'	5017.56'
⑩	782081.507'	757480.039'	5017.56'
⑪	782077.506'	757480.055'	5017.61'
⑫	782073.488'	757480.071'	5017.66'
⑬	782081.520'	757483.185'	5017.64'
⑭	782080.669'	757484.042'	5017.67'
⑮	782073.504'	757484.071'	5017.77'
⑯	782081.545'	757489.185'	5017.37'
⑰	782079.545'	757489.193'	5017.53'
⑱	782075.545'	757489.210'	5017.85'
⑲	782073.526'	757489.218'	5017.88'
⑳	782073.517'	757487.194'	5017.85'
㉑	782071.534'	757486.635'	5017.80'
㉒	782071.545'	757489.226'	5017.91'
㉓	782071.561'	757493.226'	5017.98'
㉔	782075.561'	757493.210'	5017.91'
㉕	782081.586'	757499.185'	5017.81'
㉖	782081.597'	757501.807'	5017.82'
㉗	782073.593'	757501.839'	5017.94'
㉘	782079.561'	757493.193'	5017.58'
㉙	782081.561'	757493.185'	5017.41'
㉚	782077.523'	757484.055'	5017.71'

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	J. ALLEN	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED	B. BURKE	
DETAILED	A. HUNTSMAN	CADD FILE NAME 24346_CR_Det.DGN
DRAWING CHECKED	B. BURKE	DRAWING DATE: November 14, 2024

**IDAHO
TRANSPORTATION
DEPARTMENT**



Horrocks.

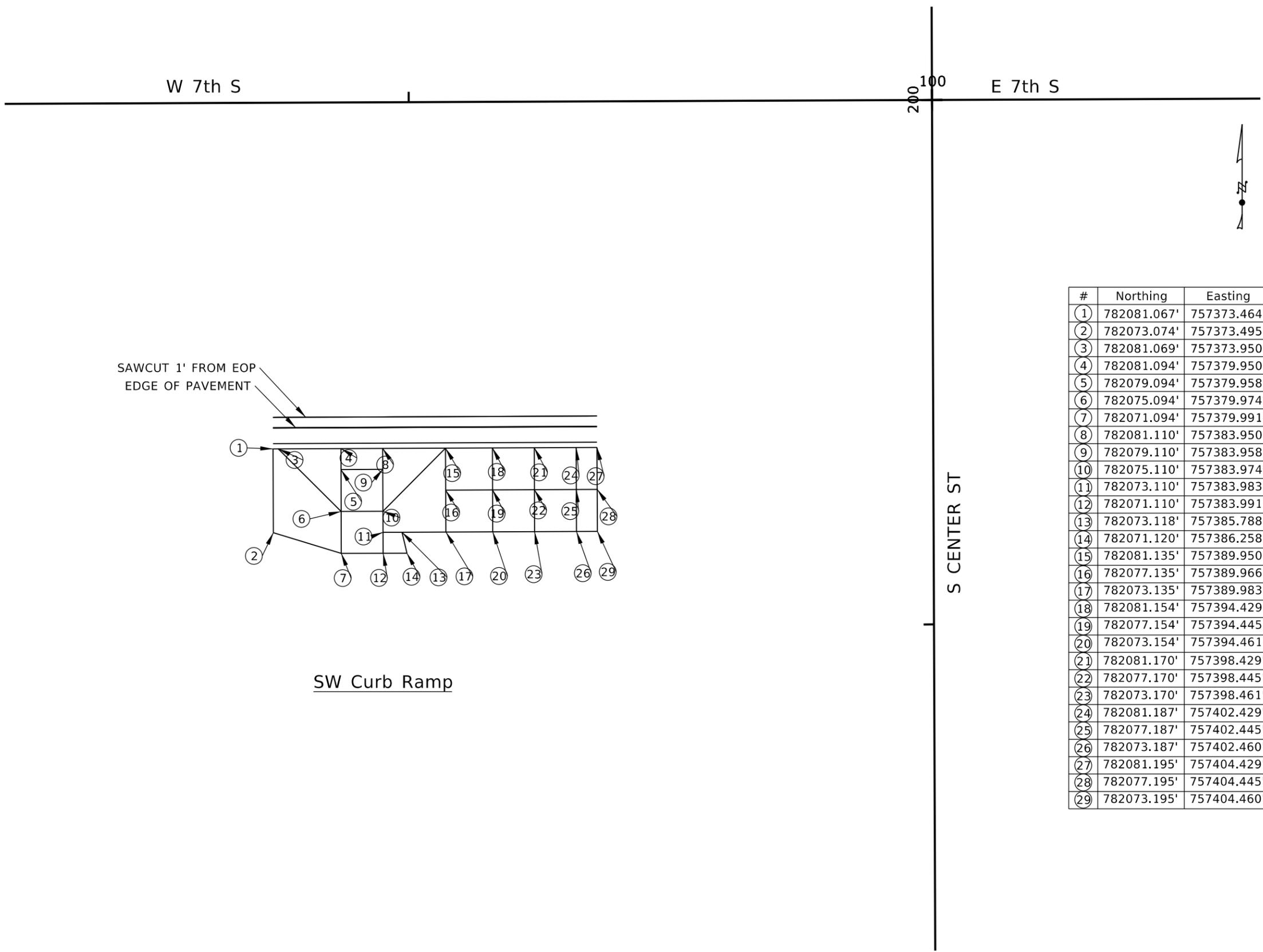
PROJECT NO.	A024(346)
-------------	-----------

CURB DETAIL SHEET

**SMA-7866, INT 7TH SOUTH
& CENTER ST
SIGNAL, REXBURG**

ENGLISH	
COUNTY	MADISON
KEY NUMBER	24346
SHEET 9 OF 34	

ORIGINAL STORED
AT: ITD, District 6
5151 South 5th,
Pocatello, ID



#	Northing	Easting	Elevation
①	782081.067'	757373.464'	5015.39'
②	782073.074'	757373.495'	5015.49'
③	782081.069'	757373.950'	5015.41'
④	782081.094'	757379.950'	5015.28'
⑤	782079.094'	757379.958'	5015.44'
⑥	782075.094'	757379.974'	5015.77'
⑦	782071.094'	757379.991'	5015.83'
⑧	782081.110'	757383.950'	5015.35'
⑨	782079.110'	757383.958'	5015.51'
⑩	782075.110'	757383.974'	5015.83'
⑪	782073.110'	757383.983'	5015.85'
⑫	782071.110'	757383.991'	5015.87'
⑬	782073.118'	757385.788'	5015.85'
⑭	782071.120'	757386.258'	5015.99'
⑮	782081.135'	757389.950'	5015.88'
⑯	782077.135'	757389.966'	5015.92'
⑰	782073.135'	757389.983'	5015.96'
⑱	782081.154'	757394.429'	5016.01'
⑲	782077.154'	757394.445'	5016.03'
⑳	782073.154'	757394.461'	5016.06'
㉑	782081.170'	757398.429'	5016.12'
㉒	782077.170'	757398.445'	5016.14'
㉓	782073.170'	757398.461'	5016.16'
㉔	782081.187'	757402.429'	5016.08'
㉕	782077.187'	757402.445'	5016.17'
㉖	782073.187'	757402.460'	5016.25'
㉗	782081.195'	757404.429'	5016.06'
㉘	782077.195'	757404.445'	5016.18'
㉙	782073.195'	757404.460'	5016.30'

SAWCUT 1' FROM EOP
EDGE OF PAVEMENT

SW Curb Ramp

S CENTER ST

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED J. ALLEN
 DESIGN CHECKED B. BURKE
 DETAILED A. HUNTSMAN
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_CR_Det.DGN
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT



Horrocks.

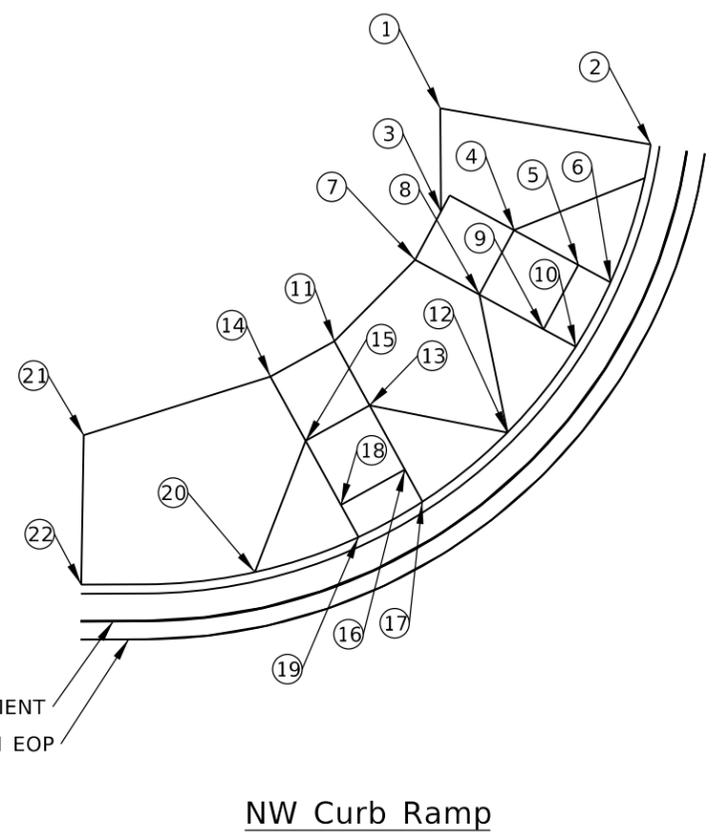
PROJECT NO. A024(346)

CURB DETAIL SHEET
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 10 OF 34

ORIGINAL STORED AT: ITD, District 6 5151 South 5th, Pocatello, ID





S CENTER St



#	Northing	Easting	Elevation
①	782173.025'	757384.399'	5015.09'
②	782171.045'	757395.907'	5015.03'
③	782168.277'	757384.916'	5015.13'
④	782166.375'	757388.434'	5015.07'
⑤	782164.472'	757391.953'	5014.79'
⑥	782163.521'	757393.712'	5014.65'
⑦	782164.762'	757383.015'	5015.06'
⑧	782162.859'	757386.534'	5015.00'
⑨	782160.957'	757390.052'	5014.75'
⑩	782160.006'	757391.812'	5014.63'
⑪	782160.309'	757378.598'	5014.90'
⑫	782155.327'	757388.073'	5014.90'
⑬	782156.806'	757380.529'	5014.84'
⑭	782158.380'	757375.098'	5014.83'
⑮	782154.877'	757377.029'	5014.77'
⑯	782153.303'	757382.460'	5014.57'
⑰	782151.551'	757383.425'	5014.43'
⑱	782151.374'	757378.960'	5014.52'
⑲	782149.622'	757379.925'	5014.40'
⑳	782147.709'	757374.250'	5014.67'
㉑	782155.179'	757364.883'	5014.56'
㉒	782147.032'	757364.746'	5014.41'

W 7th S

200 100

E 7th S

ORIGINAL STORED
AT: ITD, District 6
5151 South 5th,
Pocatello, ID

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED J. ALLEN
DESIGN CHECKED B. BURKE
DETAILED A. HUNTSMAN
DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME 24346_CR_Det.DGN
DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT



Horrocks.

PROJECT NO.
A024(346)

CURB DETAIL SHEET
SMA-7866, INT 7TH SOUTH & CENTER ST
SIGNAL, REXBURG

ENGLISH
COUNTY MADISON
KEY NUMBER 24346
SHEET 11 OF 34

ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION	ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION																																		
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR SPECIAL INSTALL SIGN (BID ITEM 616-090A)</u>			<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR SIGNAL HOUSING (BID ITEM 656-010A)</u>																																		
1.	VARIOUS	<p><u>SIGNS SUPPLIED BY THE CONTRACTOR:</u></p> <table style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>SIGN</th> <th>SIZE (INCHES)</th> <th>MATERIAL</th> <th>AREA (SF)</th> <th>QTY</th> <th>TOTAL SF</th> </tr> </thead> <tbody> <tr> <td>D3-1</td> <td>24 X 102</td> <td>B-1</td> <td>17.0</td> <td>2</td> <td>34.0</td> </tr> <tr> <td>D3-1</td> <td>24 X 96</td> <td>B-1</td> <td>16.0</td> <td>2</td> <td>32.0</td> </tr> </tbody> </table>	SIGN	SIZE (INCHES)	MATERIAL	AREA (SF)	QTY	TOTAL SF	D3-1	24 X 102	B-1	17.0	2	34.0	D3-1	24 X 96	B-1	16.0	2	32.0	1.	10 EACH	<p>VEHICLE SIGNAL HEADS, 12", VERTICAL, 3 SECTION, WITH 5" FLAT BLACK POLYCARBONATE BACKPLATE WITH 2" REFLECTIVE SHEETING AND WITH FLAT BLACK POLYCARBONATE TUNNEL VISORS, FOR ALL TRAFFIC SIGNAL HEAD NUMBERS 21, 22, 23, 41, 42, 61, 62, 63, 81 & 82. TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ORIUX CATALOG NO. PSS83C003FO, NO. 0700493C AND NO. S400013 MCCAIN TRAFFIC SUPPLY CATALOG NO. M31515, NO. M19273 AND NO. M18216 MOBOTREX CATALOG NO. SG103A1C11GGG30, NO. A-700120-03 AND NO. BPB503ANF022E 																
SIGN	SIZE (INCHES)	MATERIAL	AREA (SF)	QTY	TOTAL SF																																		
D3-1	24 X 102	B-1	17.0	2	34.0																																		
D3-1	24 X 96	B-1	16.0	2	32.0																																		
2.	2 EACH	<p><u>SIGN MOUNTING BRACKETS</u>, FOR 24" X 102" STREET NAME SIGNS, TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ASTRO BRAC PELCO MODEL NO. AS-0144-96-120-SS OLSON SKYBRAC MODEL NO. SS-SBC120-SBK-96TK-23 	2.	4 EACH	<p>VEHICLE SIGNAL HEADS, 12", VERTICAL, 4 SECTION, WITH 5" FLAT BLACK POLYCARBONATE BACKPLATE WITH 2" REFLECTIVE SHEETING AND WITH FLAT BLACK POLYCARBONATE TUNNEL VISORS, FOR ALL TRAFFIC SIGNAL HEAD NUMBERS 11, 31, 51 & 71. TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ORIUX CATALOG NO. PSS884004FO, NO. 0700493C AND NO. S400014 MCCAIN TRAFFIC SUPPLY CATALOG NO. M31515, NO. M19273 AND NO. M18216 MOBOTREX CATALOG NO. SG103A1C11GGG40, NO. A-700120-03 AND NO. BPB504ANF022E 																																		
3.	2 EACH	<p><u>SIGN MOUNTING BRACKETS</u>, FOR 24" X 96" STREET NAME SIGNS, TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ASTRO BRAC PELCO MODEL NO. AS-0144-90-120-SS OLSON SKYBRAC MODEL NO. SS-SBC120-SBK-90TK-23 	3.	6 EACH	<p><u>TRAFFIC SIGNAL BRACKET</u>, FOR MAST ARM MOUNTING OF THREE-SECTION HEAD TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ASTRO BRAC PELCO MODEL NO. AS-0125-3-96 OLSON SS-SBC(90)-46 OR CORAL SS-SBC(90)-46 																																		
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR ILLUMINATION TYPE 2 SIGNALS (BID ITEM 619-010A)</u>																																					
1.	4 EACH	<p><u>LED LUMINAIRE</u>, 120-227V, 17,000 TO 19,000 LUMENS, 4000K, TYPE II, FULL CUT-OFF, TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> AMERICAN ELECTRIC LIGHTING NO. ATBO-P304-MVOLT-R2-A0-P7-SH-10X EATON STREETWORKS NAVION NO. NVN-SA3B-740-U-T2R-10K-AP-PR7-OA/RA1013 WITH RFWD-10 YEARS EATON STREETWORKS ARCHEON MEDIUM NO. ARCH-M-PA2-120-740-U-T2-10K-AP-PR7-OA/RA1013 WITH FWD-10 YEARS 	4.	4 EACH	<p><u>TRAFFIC SIGNAL BRACKET</u>, FOR SIDE OF POLE MOUNTING OF ONE THREE-SECTION HEAD TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> MCCAIN TRAFFIC SUPPLY CATALOG NO. SV-1-T PELCO CONTROL PRODUCTS SP-8357-3-LH-WH-AD-P34 																																		
2.	8 EACH	<p><u>FUSE</u>, 8 AMP CARTRIDGE TYPE TO BE BUSSMAN MFG CATALOG NO. FNM-8, LITTELFUSE FLM-8M, FERRAZ SHAWMUT CAT NO. TRM-8 OR APPROVED EQUAL</p>	5.	4 EACH	<p><u>TRAFFIC SIGNAL BRACKET</u>, FOR MAST ARM MOUNTING OF FOUR-SECTION HEAD TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <ol style="list-style-type: none"> ASTRO BRAC PELCO MODEL NO. AS-0125-4-96 OLSON SS-SBC(90)-46 OR CORAL SS-SBC(90)-60 																																		
3.	8 EACH	<p><u>SINGLE POLE CONNECTOR KITS</u>, (2 KITS PER LUMINAIRE), INLINE FUSED TYPE TO BE BUSSMAN MODEL NO. HEB-JW-RLC-A; LITTELFUSE LED-AA-S; BUCHANAN 65-U-U, OR APPROVED EQUAL</p>	6.		<p><u>12" LED TRAFFIC SIGNAL MODULES</u>, WITH QUICK DISCONNECT LEADS, SMOOTH LENS SURFACE, 120 VAC. TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL:</p> <table style="width:100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th>GE LIGHTING</th> <th>DIALIGHT</th> <th>LEOTEK</th> </tr> </thead> <tbody> <tr> <td>10 EACH</td> <td>LED RED BALL</td> <td>DR6-RTFB-VLA</td> <td>433-1210-003</td> <td>TSL-12R-LX-IL6-A1-P3</td> </tr> <tr> <td>10 EACH</td> <td>LED YELLOW BALL</td> <td>DR6-YTFB-VLA</td> <td>433-3230-901</td> <td>TSL-12Y-LX-IL6-A1-P3</td> </tr> <tr> <td>10 EACH</td> <td>LED GREEN BALL</td> <td>DR6-GCFB-VLA</td> <td>433-2270-001</td> <td>TSL-12G-LX-IL6-A1-P3-CLR</td> </tr> <tr> <td>4 EACH</td> <td>LED RED ARROW</td> <td>DR6-RTAAN-VLA</td> <td>432-1314-001</td> <td>TSL-12RA-LX-IL6-A1-P3</td> </tr> <tr> <td>8 EACH</td> <td>LED YELLOW ARROW</td> <td>DR6-YTAAAN-VLA</td> <td>431-3334-901</td> <td>TSL-12YA-LX-IL6-A1-P3</td> </tr> <tr> <td>4 EACH</td> <td>LED GREEN ARROW</td> <td>DR6-GCAAN-VLA</td> <td>432-2374-001</td> <td>TSL-12GA-LX-IL6-A1-P3-CLR</td> </tr> </tbody> </table>		GE LIGHTING	DIALIGHT	LEOTEK	10 EACH	LED RED BALL	DR6-RTFB-VLA	433-1210-003	TSL-12R-LX-IL6-A1-P3	10 EACH	LED YELLOW BALL	DR6-YTFB-VLA	433-3230-901	TSL-12Y-LX-IL6-A1-P3	10 EACH	LED GREEN BALL	DR6-GCFB-VLA	433-2270-001	TSL-12G-LX-IL6-A1-P3-CLR	4 EACH	LED RED ARROW	DR6-RTAAN-VLA	432-1314-001	TSL-12RA-LX-IL6-A1-P3	8 EACH	LED YELLOW ARROW	DR6-YTAAAN-VLA	431-3334-901	TSL-12YA-LX-IL6-A1-P3	4 EACH	LED GREEN ARROW	DR6-GCAAN-VLA	432-2374-001	TSL-12GA-LX-IL6-A1-P3-CLR
	GE LIGHTING	DIALIGHT	LEOTEK																																				
10 EACH	LED RED BALL	DR6-RTFB-VLA	433-1210-003	TSL-12R-LX-IL6-A1-P3																																			
10 EACH	LED YELLOW BALL	DR6-YTFB-VLA	433-3230-901	TSL-12Y-LX-IL6-A1-P3																																			
10 EACH	LED GREEN BALL	DR6-GCFB-VLA	433-2270-001	TSL-12G-LX-IL6-A1-P3-CLR																																			
4 EACH	LED RED ARROW	DR6-RTAAN-VLA	432-1314-001	TSL-12RA-LX-IL6-A1-P3																																			
8 EACH	LED YELLOW ARROW	DR6-YTAAAN-VLA	431-3334-901	TSL-12YA-LX-IL6-A1-P3																																			
4 EACH	LED GREEN ARROW	DR6-GCAAN-VLA	432-2374-001	TSL-12GA-LX-IL6-A1-P3-CLR																																			
4.	12 EACH	<p><u>SPLICE KITS</u>, 3 KITS PER LUMINAIRE, TO BE MECHANICAL WATERPROOF</p>																																					

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED B. BURKE	CADD FILE NAME 24346_Mat_001.dgn
DETAILED R. TELFORD	DRAWING DATE: November 14, 2024
DRAWING CHECKED B. BURKE	

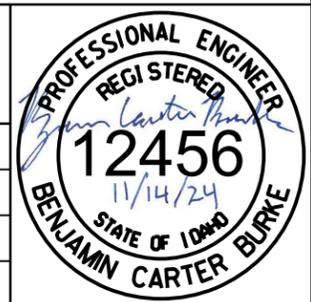
IDAHO
TRANSPORTATION
DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

PROJECT NO.	A024(346)
-------------	-----------

MATERIALS QUANTITY SHEET	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
--------------------------	---

ENGLISH	COUNTY MADISON
KEY NUMBER	24346
SHEET	12 OF 34



ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION
<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR INTERSECTION DETECTION SYSTEM VIDEO (BID ITEM 656-020C)</u>		
1.	1 EACH	<u>CURREX VIDEO DETECTION SYSTEM</u> , TO INCLUDE 1 AXIS DETECTION SYSTEM WITH THE FOLLOWING SET UP, CAMERA M3058-PLVE FIXED DOME 12 MP WDR MULTISTREAM 360/180 DEG, POE, POWER SUPPLY (T8134), HIGH POE MIDSPAN 60 W, INDOOR PENDANT KIT FOR P32-V, P33-V, Q3505-V, AND SKY BRACKET PENDANT CAMERA MOUNT WITH 59" SS BANDING. TO ALSO INCLUDE 1 CURREX VISION DETECTION SYSTEMS WITH THE FOLLOWING MODULES, SMARTCITY ITS STANDARD, ANALOG CAMERA ADAPTER (4CH), SDLC MODULE (SERIAL VEH/PED/BIKE DETECTION) AND THE NTCIP MODULE.
2.	VARIOUS	<u>MISCELLANEOUS ITEMS</u> , TO INCLUDE WIRING, CONNECTORS, HARDWARE, FITTINGS AND VARIOUS OTHER ITEMS NECESSARY FOR A COMPLETE INSTALLATION. PLACE A #12 LOCATE WIRE WITH VIDEO DETECTION CONDUIT.
<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR ELECTRICAL SERVICE PEDESTAL TYPE 1 (BID ITEM 656-025A)</u>		
1.	1 EACH	<u>ELECTRICAL SERVICE PEDESTAL</u> , TYPE 1, WITH PHOTO CELL CONTROL TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL: 1. MYERS ELECTRICAL PRODUCTS. CATALOG NO. MEUG20-M100w/TB-ITD (MOD) 2. MILBANK NO. CP3B11113BWID01
2.	1 EACH	<u>PHOTOELECTRIC CONTROL</u> , 240 VOLT TO BE MOUNTED ON RECEPTACLE PROVIDED IN ELECTRICAL SERVICE PEDESTAL. TO BE INTERMATIC MODEL NO. LED4536SC, FP OUTDOOR LIGHTING CONTROL MODEL NO. ELL-124, TORK-NSI MODEL NO ZTL124 OR APPROVED EQUAL
<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR INTERSECTION SIGNAL STRUCTURE (BID ITEM 656-030A)</u>		
1.	1 EACH	<u>POLE</u> , STEEL, COMBINATION TRAFFIC SIGNAL AND LUMINAIRE, WITH 40' SIGNAL MASTARM AND A 15' LUMINAIRE MASTARM WITH 2" SLIP FITTER (LUMINAIRE MOUNTING HEIGHT TO BE 40'). TO BE POWDER COATED BROWN, FINAL COLOR TO BE APPROVED BY THE CITY OF REXBURG. TO BE CLASS 2 STANDARD MAST ARM TRAFFIC SIGNAL POLE (IDENTIFICATION NO. ID-2-40-L-4015) SUPPLIED BY VALMONT, AMERON OR APPROVED EQUAL. FOR POLES "A" AS INDICATED ON PLANS. REQUIRES TYPE "D" FOUNDATION.
2.	2 EACH	<u>POLE</u> , STEEL, COMBINATION TRAFFIC SIGNAL AND LUMINAIRE, WITH A 45' SIGNAL MASTARM AND A 15' LUMINAIRE MASTARM WITH 2" SLIP FITTER (LUMINAIRE MOUNTING HEIGHT TO BE 40'). TO BE POWDER COATED BROWN, FINAL COLOR TO BE APPROVED BY THE CITY OF REXBURG. TO BE CLASS 2 STANDARD MAST ARM TRAFFIC SIGNAL POLE (IDENTIFICATION NO. ID-2-45-L-4015) SUPPLIED BY VALMONT, AMERON OR APPROVED EQUAL. FOR POLES "B AND D" AS INDICATED ON PLANS. REQUIRES TYPE "D" FOUNDATION.
3.	1 EACH	<u>POLE</u> , STEEL, COMBINATION TRAFFIC SIGNAL AND LUMINAIRE, WITH A 50' SIGNAL MASTARM AND A 15' LUMINAIRE MASTARM WITH 2" SLIP FITTER (LUMINAIRE MOUNTING HEIGHT TO BE 40'). TO BE POWDER COATED BROWN, FINAL COLOR TO BE APPROVED BY THE CITY OF REXBURG. TO BE CLASS 3 STANDARD MAST ARM TRAFFIC SIGNAL POLE (IDENTIFICATION NO. ID-3-50-L-4015) SUPPLIED BY VALMONT, AMERON OR APPROVED EQUAL. FOR POLES "C" AS INDICATED ON PLANS. REQUIRES TYPE "F" FOUNDATION.
4.	3 EACH	<u>FOUNDATION</u> , REINFORCED CONCRETE, TYPE D FOR SIGNAL POLE. SEE ITD STANDARD DRAWING 656-3 FOR BOLT PATTERN, REBAR QUANTITY, AND CONCRETE QUANTITY.
5.	1 EACH	<u>FOUNDATION</u> , REINFORCED CONCRETE, TYPE F FOR SIGNAL POLE. SEE ITD STANDARD DRAWING 656-3 FOR BOLT PATTERN, REBAR QUANTITY, AND CONCRETE QUANTITY.

ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION
<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR INTERSECTION PEDESTRIAN SYSTEM (BID ITEM 656-035A)</u>		
1.	8 EACH	<u>PEDESTRIAN SIGNAL BRACKET</u> , FOR SIDE OF POLE TERMINAL COMPARTMENT MOUNTING OF ONE WAY 16" PEDESTRIAN SIGNAL HEADS. TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL: 1. MCCAIN TRAFFIC SUPPLY CATALOG NO. SP-1-T 2. PELCO CONTROL PRODUCTS SP-8317-LH-WH-AD-P34
2.	8 EACH	<u>PEDESTRIAN SIGNAL HOUSING</u> , 18" X 18", TRAFFIC OLIVE GREEN, WITH POWDER-COATED ALUMINUM EXTERIOR UV RESISTANT POLYCARBONATE FOR ALL PEDESTRIAN SIGNAL HEADS. TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL: 1. ECONOLITE 2. MOBOTREX 3. MCCAIN
3.	8 EACH	<u>PEDESTRIAN SIGNAL HEAD</u> , 16", LED COUNTDOWN HAND/MAN, TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL: 1. DIALIGHT 430-6479-001X 2. LEOTEK TSL-PED-16-CIL-P1
4.	8 EACH	<u>PEDESTRIAN PUSH BUTTON AND SIGN ASSEMBLY</u> , TO INCLUDE 9" X 15" MOUNTING ASSEMBLIES, PUSH BUTTON AND 9" X 15" SIGN DECALS (MUTCD R10-3E) TO BE: 1. POLARA ENGINEERING MODEL IN23TN0-B 2-WIRE
5.	1 EACH	<u>PEDESTRIAN PUSH BUTTON MOUNTING EXTENDER</u> , WELDED STEEL WITH A HOT DIPPED GALVANIZED COATING. TO INCLUDE 6" EXTENSION AND ALL HARDWARE NECESSARY FOR INSTALLATION, FOR POLE "A". TO BE: 1. POLARA ENGINEERING MODEL IN-EXT-06
<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR SIGNAL CABINET (BID ITEM 656-040A)</u>		
1.	1 EACH	<u>SIGNAL CABINET</u> , TS-2 TYPE 1, NEMA "P" STRETCH, ALUMINUM TRAFFIC CONTROL CABINET, WITH ALL RELATED EQUIPMENT, RELAYS, MALFUNCTION MANAGEMENT UNIT, 16 LOAD SWITCHES, DETECTOR RACK FULLY CONFIGURED FOR CURREX SYSTEM WITH AXIS CAMERA AND 4 EMERGENCY PREEMPT DETECTORS. THE CABINET SHALL BE CONFIGURED WITH POWER DISTRIBUTION, POLICE PANEL, ETHERNET SWITCH, TECH PANEL AND TEST SWITCHES PER IDAHO STANDARDS AND SPECIFICATIONS FOR A TS-2 TYPE 1 P-A-16 CABINET AVAILABLE ON ITD'S WEBSITE IN REFERENCE AND STANDARDS. MMU SHALL BE EDI. THE ETHERNET SWITCH SHALL BE SIEMENS RUGGEDCOM RS900G. THE ONT SHALL BE CALIX GP4200XH PART NUMBER 100-05589.
2.	1 EACH	<u>BATTERY BACKUP SYSTEM</u> , TO BE UPS WITH AMBIENT TEMPERATURE AA ENCLOSURE COMPLETE WITH FOUR 12-VOLT BATTERIES AND FULL LED 3-HOUR RUN TIME.

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED B. BURKE	CADD FILE NAME 24346_Mat_002.dgn
DETAILED R. TELFORD	DRAWING DATE: November 14, 2024
DRAWING CHECKED B. BURKE	

IDAHO TRANSPORTATION DEPARTMENT
YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

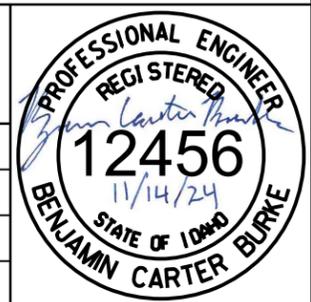


Horrocks.

PROJECT NO.	A024(346)
-------------	-----------

MATERIALS QUANTITY SHEET	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
--------------------------	---

ENGLISH	COUNTY MADISON
KEY NUMBER	24346
SHEET	13 OF 34



ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION	ITEM NO.	QUAN.	7TH SOUTH AND CENTER ST INTERSECTION
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR SIGNAL CONTROLLER (BID ITEM 656-045A)</u>	7.	4 EACH	<u>SMFO SC FIBER OPTIC PATCH CORDS</u> , 1 METER LONG, 3 MILLIMETERS WITH FACTORY INSTALLED CONNECTOR.
1.	1 EACH	<u>CONTROLLER</u> , SHALL BE A SEIMENS M62.	8.	VARIOUS	<u>MISCELLANEOUS</u> ITEMS TO INCLUDE: RIGID STEEL AND RIGID PLASTIC CONDUIT: CONDUIT TO BE SCH 40 SMOOTH WALL DUCT, #2/0 AWG THWN STRANDED COPPER WIRE (600V); #4 AWG THWN STRANDED COPPER WIRE (600V); #8 AWG THWN STRANDED WIRE (600V); #12 AWG THWN STRANDED WIRE (600V); #14 AWG 7C SIGNAL CABLE (IMSA 19-1-1984); #14 5C SIGNAL CABLE (IMSA 19-1-1984) CORNING SMFO 12 COUNT LOOSE TUBER FIBER.
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR PEDESTRIAN AND TRAFFIC CONTROL SIGNAL HEAD FRANGIBLE POLES (BID ITEM 656-050A)</u>			
1.	7 EACH	<u>POLE</u> , STEEL, VEH. AND PEDESTRIAN SIGNAL 4-INCH, POWDER COATED BROWN RIGID STEEL CONDUIT AT A 4' HEIGHT, THREADED TO MATCH PELCO FRANGIBLE CAST BASE FOR POLES "E, F, G, H, I, J AND K". REQUIRES TYPE "E" FOUNDATION.			
3.	7 EACH	<u>FOUNDATION</u> , REINFORCED CONCRETE, TYPE E FOR PUSH BUTTON POLE. SEE ITD STANDARD DRAWING 656-3 FOR BOLT PATTERN, REBAR QUANTITY, AND CONCRETE QUANTITY.			
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR COMBINED CABINET & SERVICE PEDESTAL FOUNDATION (BID ITEM 656-065A)</u>			
1.	1 EACH	<u>FOUNDATION</u> , REINFORCED CONCRETE, FOR COMBINATION SIGNAL CABINET, SERVICE PEDESTAL, AND UPS CABINET WITH CONCRETE PAD.			
		<u>ITEMS TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR ADDITIONAL ELECTRICAL ITEMS (BID ITEM 656-070A)</u>			
1.	2 EACH	<u>JUNCTION BOXES</u> , POLYMER CONCRETE SIZE 17" X 30" X 18" WITH TRAFFIC RATED LOCKING COVER WITH "TRAFFIC" LOGO TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL" 1. HUBBELL NO. PG1730Z854 2. OLD CASTLE NO. PC17x30x18-T22 3. ARMORCAST NO. A6001640HDAPCX18			
2.	11 EACH	<u>JUNCTION BOXES</u> , POLYMER CONCRETE SIZE 13" X 24" X 18" WITH TRAFFIC RATED LOCKING COVER WITH "TRAFFIC" LOGO TO BE ONE OF THE FOLLOWING OR APPROVED EQUAL" 1. HUBBELL NO. PG1324BA18 2. OLD CASTLE NO. S1324B18FA 3. ARMORCAST NO. A6001946HDAPCX18			
3.	4 EACH	<u>FIBER OPTIC FUSION SPLICE</u> , SHALL HAVE A DB LOSS LESS THAN 0.5, WITH A HEAT SHRINK PROTECTOR. SHALL BE OTDR TESTED IN BOTH DIRECTION USING A 1 KM JUMP BOX. SUBMIT TEST RESULTS TO THE ENGINEER FOR APPROVAL.			
4.	1 EACH	<u>FIBER OPTIC SPLICE ENCLOSURE</u> , TO BE SHELF MOUNTED AND HOLD 1-12COUNT SPLICE TRAYS. SHALL BE CALIX ONT CMN-SFU ECNL-ST, PART NUMBER 100-01578.			
5.	1 EACH	<u>FIBER OPTIC PATCH PANEL</u> , TO BE SHELF MOUNTED WITH 12 SC PORTS			
6.	2 EACH	<u>SMFO SC PIGTAILS</u> , 3 METERS LOG AND COLOR CODED TO MATCH FIBER COLOR, 900 MICRO METERS WITH FACTORY INSTALLED CONNECTOR.			

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED B. BURKE	CADD FILE NAME 24346_Mat_003.dgn
DETAILED R. TELFORD	DRAWING DATE: November 14, 2024
DRAWING CHECKED B. BURKE	

IDAHO TRANSPORTATION DEPARTMENT
YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

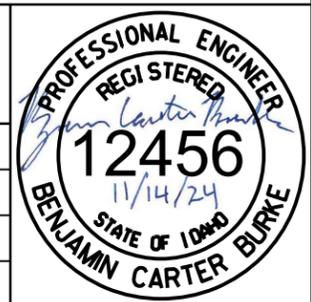


Horrocks.

PROJECT NO.	A024(346)
-------------	-----------

MATERIALS QUANTITY SHEET	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
--------------------------	---

ENGLISH	COUNTY MADISON
KEY NUMBER	24346
SHEET	14 OF 34



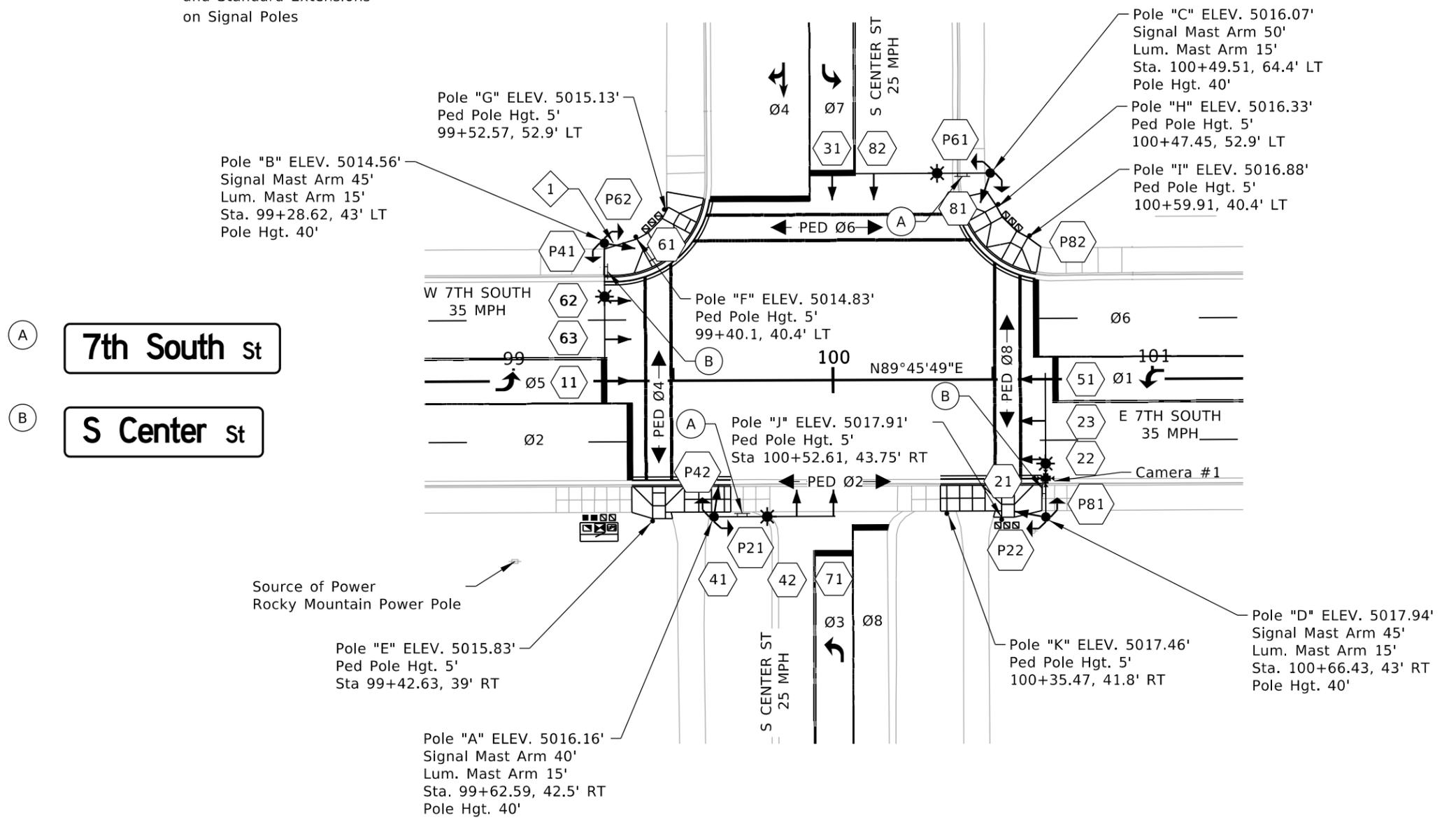
T. 6 N., R. 40 E., B.M.

LIGHTING SPECIFICATIONS

- LED
- Type 2R
- 40 Feet
- Underground Conduit
- Foundation Mounted 120/240 V
- Galvanized Steel Poles and Standard Extensions on Signal Poles

LEGEND

- Signal Pole w/ Mast Arm
- Vehicle Signal Head
- Pedestrian Signal Head
- Video Detection Camera
- Mast Arm Sign
- Luminaire w/ Mast Arm
- Signal Cabinet
- Service Pedestal
- Uninterrupted Power Service
- PGB Composite J-Box
- PGC Composite J-Box
- Signal Conduit
- Detection Conduit
- Illumination Conduit
- Wire Note
- Construction Note
- Signal Head Number
- Pedestrian Head Number



(A) **7th South st**

(B) **S Center st**

Source of Power
Rocky Mountain Power Pole



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED B BURKE
 DESIGN CHECKED R TELFORD
 DETAILED B BURKE
 DRAWING CHECKED T TELFORD

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME 24346_int_001.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

Horrocks.

PROJECT NO. A024(346)

SIGNAL INTERSECTION PLAN

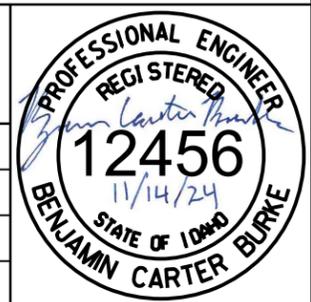
SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

COUNTY MADISON

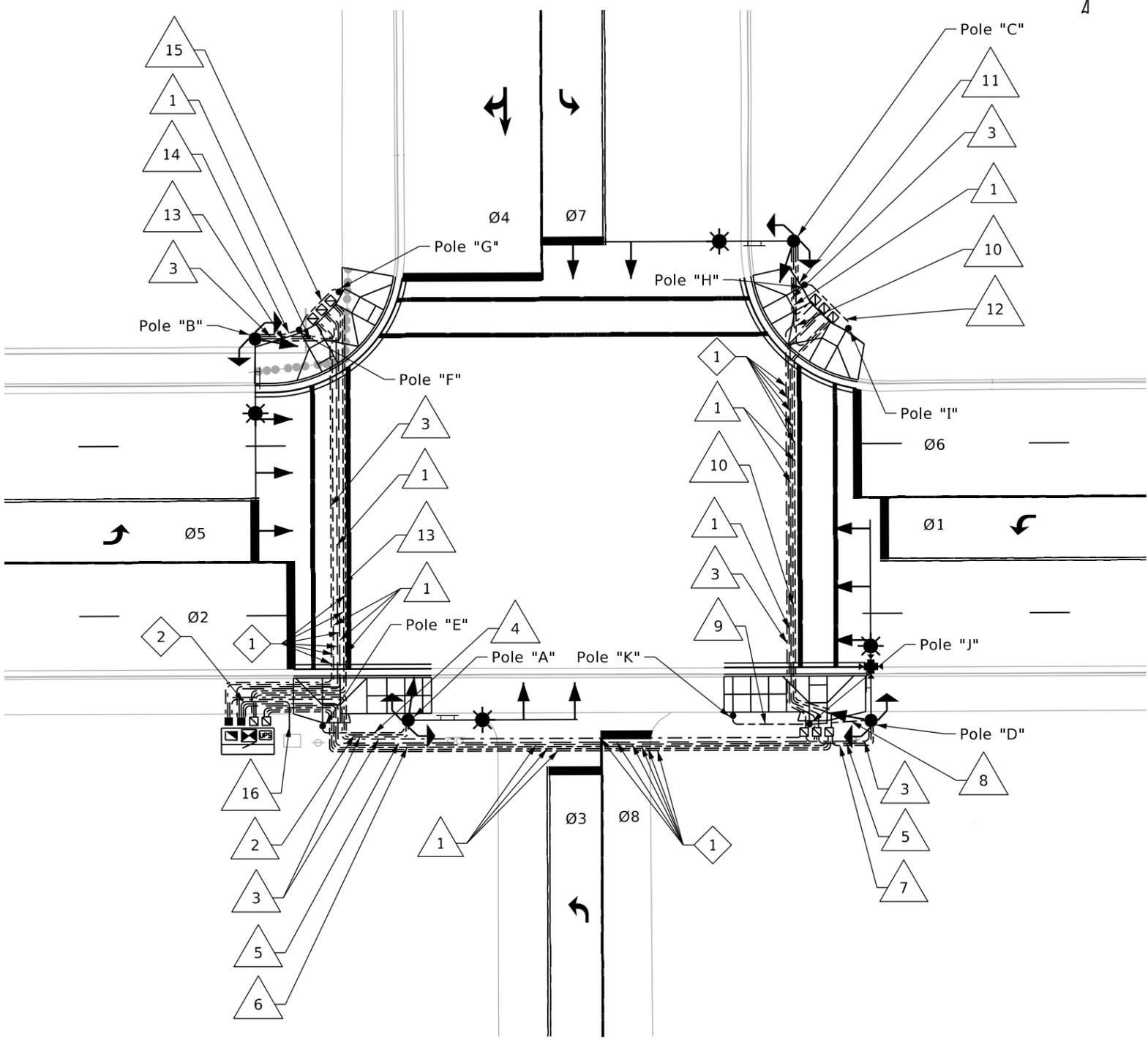
KEY NUMBER 24346

SHEET 15 OF 34



T. 6 N., R. 40 E., B.M.

Note	Conduit	Conductor	Comments
1	2" PC	Empty	Spare
2	2" PC	3-5c	Phase Ø4, ØP2 & ØP4
		1-7c	Phase Ø7
		1-#6 Bare	Ground
3	2" PC	2-#8 THWN	Circuit #1
		1-#6 Bare	Ground
4	2" PC	1-5c	Phase ØP4
		1-#6 Bare	Ground
5	2" PC	1-Cat 5E	Detection Cable Camera #1
		1-#8 THWN	Ground
6	2" PC	5-5c	Phase Ø2, Ø8, ØP2, ØP6 & ØP8
		2-7c	Phase Ø5 & Ø3
		1-#6 Bare	Ground
7	2" PC	3-5c	Phase Ø2, ØP2, & ØP8
		1-7c	Phase Ø5
		1-#6 Bare	Ground
8	2" PC	2-5c	Phase ØP2 & ØP8
		1-#6 Bare	Ground
9	2" PC	1-5c	Phase ØP2
		1-#6 Bare	Ground
10	2" PC	3-5c	Phase Ø8, ØP6, & ØP8
		1-7c	Phase Ø3
		1-#6 Bare	Ground
11	2" PC	2-5c	Phase ØP6 & ØP8
		1-#6 Bare	Ground
12	2" PC	1-5c	Phase ØP8
		1-#6 Bare	Ground
13	2" PC	3-5c	Phase Ø6, ØP4, & ØP6
		1-7c	Phase Ø1
		1-#6 Bare	Ground
14	2" PC	2-5c	Phase ØP4 & ØP6
		1-#6 Bare	Ground
15	2" PC	1-5c	Phase ØP6
		1-#6 Bare	Ground
16	2" PC	1-12 SMFO	Fiber Optic Signal Interconnect
		1-#18 AWG	Locate Wire



LEGEND

Signal Pole w/ Mast Arm	
Vehicle Signal Head	
Pedestrian Signal Head	
Video Detection Camera	
Mast Arm Sign	
Luminaire w/ Mast Arm	
Signal Cabinet	
Service Pedestal	
Uninterrupted Power Service	
PGB Composite J-Box	
PGC Composite J-Box	
Signal Conduit	
Detection Conduit	
Illumination Conduit	
Wire Note	
Construction Note	
Signal Head Number	
Pedestrian Head Number	

- Construction Notes:**
1. Install new conduit under existing pavement using directional drilling methods. This work will be incidental to 656-070A.
 2. Contact Silver Star for the coordination of Fiber Optic Signal Interconnect into Silver Star handhole. Brock Walters (307) 880-6629



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_cond_001.dgn
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

Horrocks.

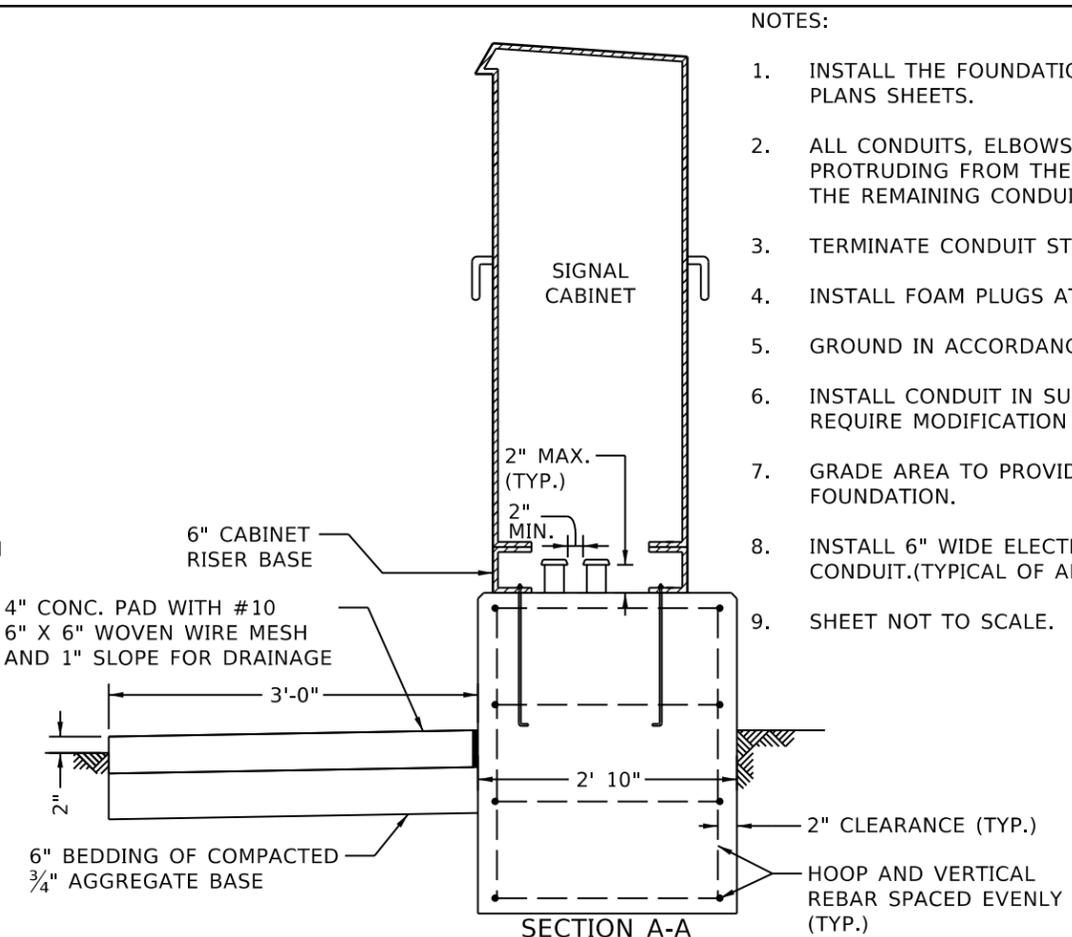
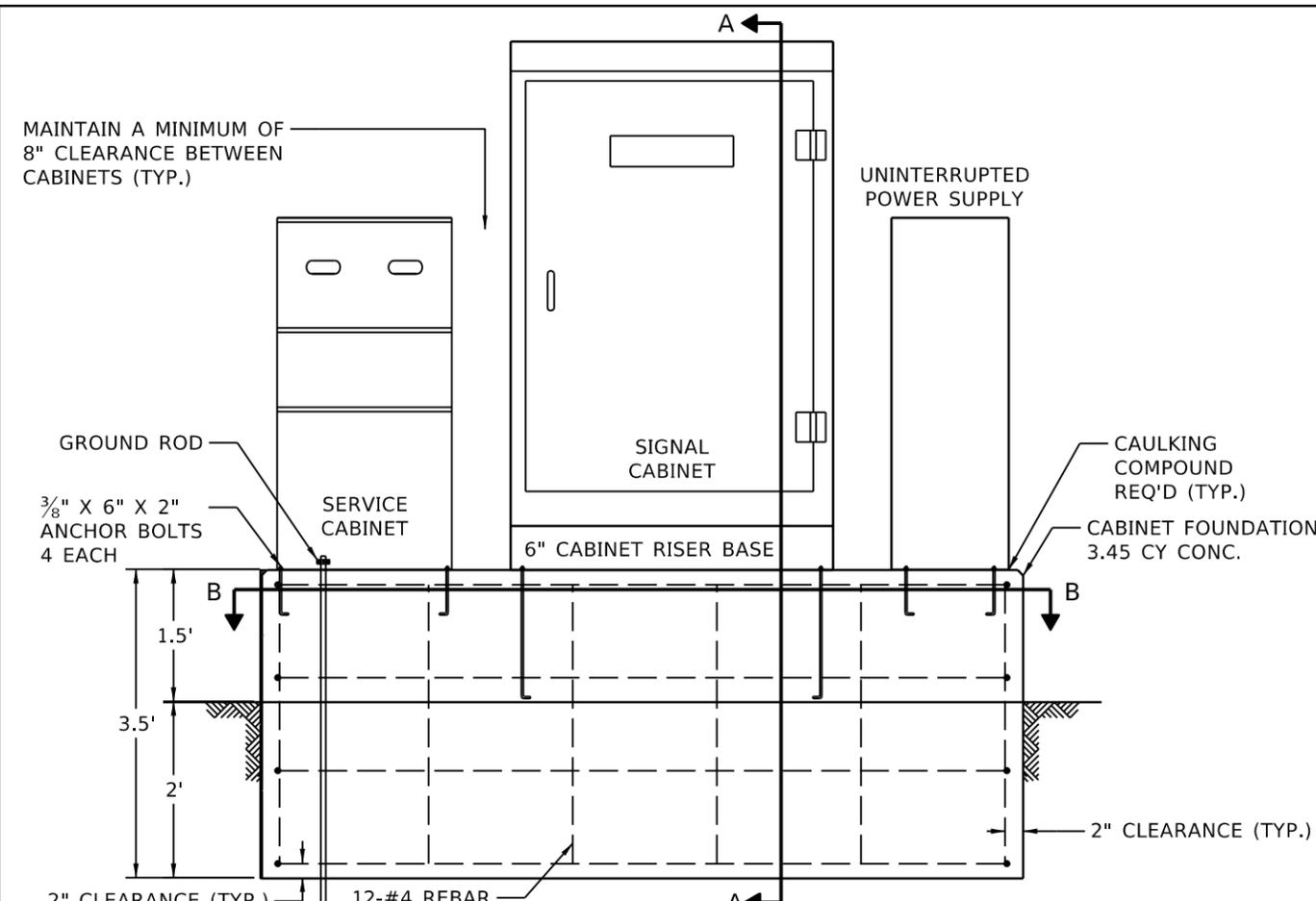
PROJECT NO. A024(346)

CONDUIT DIAGRAM
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

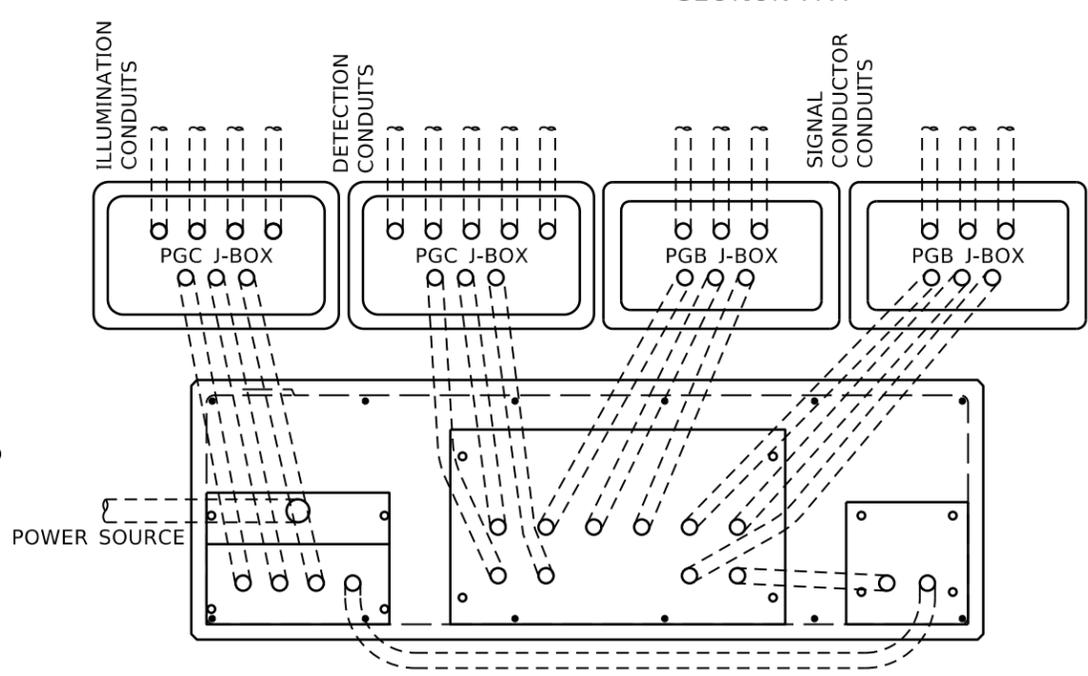
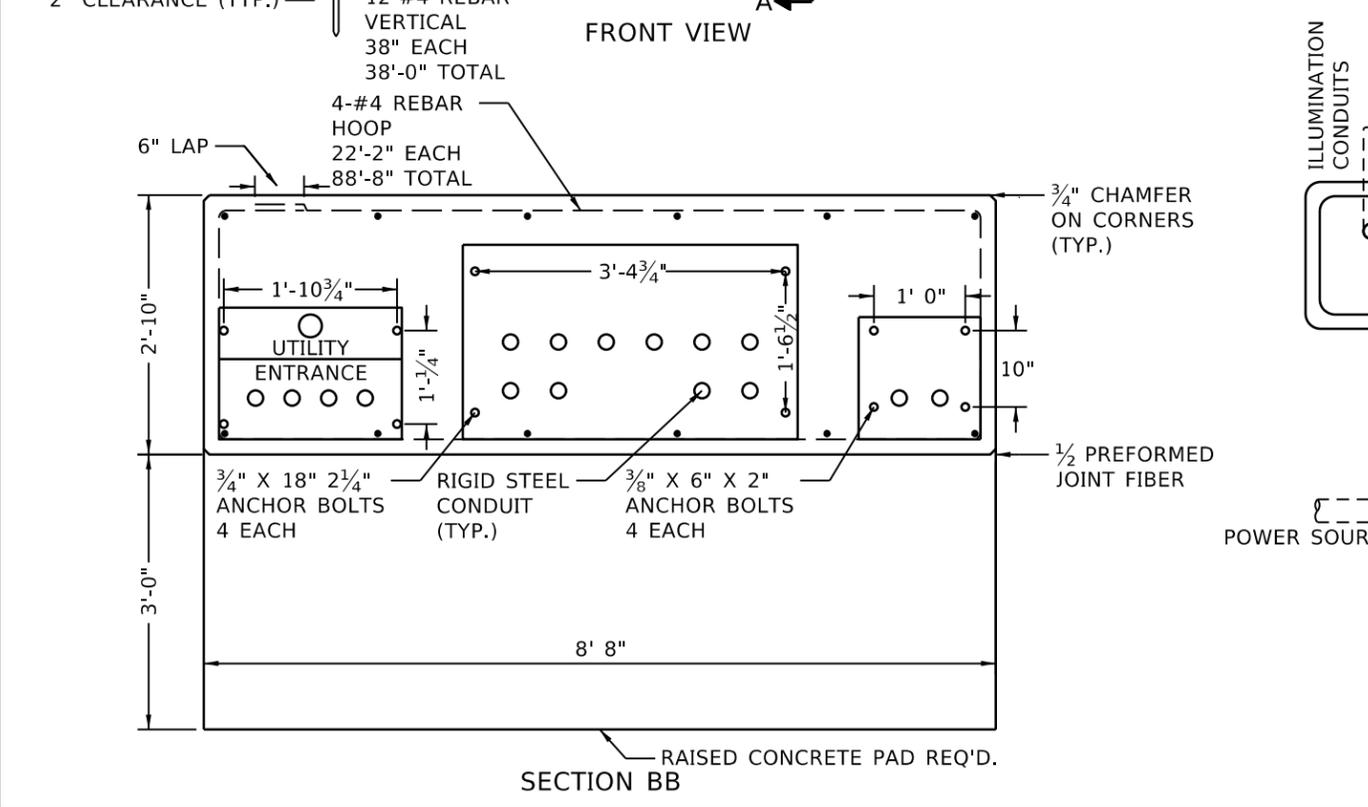
ENGLISH

COUNTY MADISON
 KEY NUMBER 24346
 SHEET 16 OF 34





- NOTES:
1. INSTALL THE FOUNDATION AS INDICATED ON THE PROJECT PLANS SHEETS.
 2. ALL CONDUITS, ELBOWS AND COUPLINGS WITHIN AND PROTRUDING FROM THE FOUNDATION SHALL BE RIGID STEEL. THE REMAINING CONDUITS SHALL BE AS SHOWN ON THE PLANS.
 3. TERMINATE CONDUIT STUBOUTS WITH A STEEL BONDING BUSHING.
 4. INSTALL FOAM PLUGS AT BOTH ENDS OF SPARE CONDUITS.
 5. GROUND IN ACCORDANCE WITH N.E.C.
 6. INSTALL CONDUIT IN SUCH A MANNER AS TO NOT CAUSE OR REQUIRE MODIFICATION OF THE CABINET.
 7. GRADE AREA TO PROVIDE DRAINAGE AWAY FORM CABINET FOUNDATION.
 8. INSTALL 6" WIDE ELECTRICAL HAZARD TAPE 1'-0" ABOVE CONDUIT.(TYPICAL OF ALL CONDUIT PLACED IN THE GROUND)
 9. SHEET NOT TO SCALE.



NOTE:

THE CONDUIT FROM THE POWER SOURCE SHALL BE 3". ALL OTHER CONDUIT SHALL BE 2". ALL CONDUITS ENCASED OR PROTRUDING FROM THE CONCRETE FOUNDATION SHALL BE RIGID STEEL.

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_cab_detail.dgn
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

Horrocks.

PROJECT NO. A024(346)

CABINET DETAILS

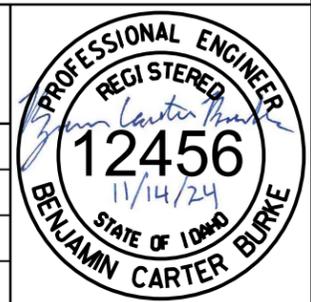
SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

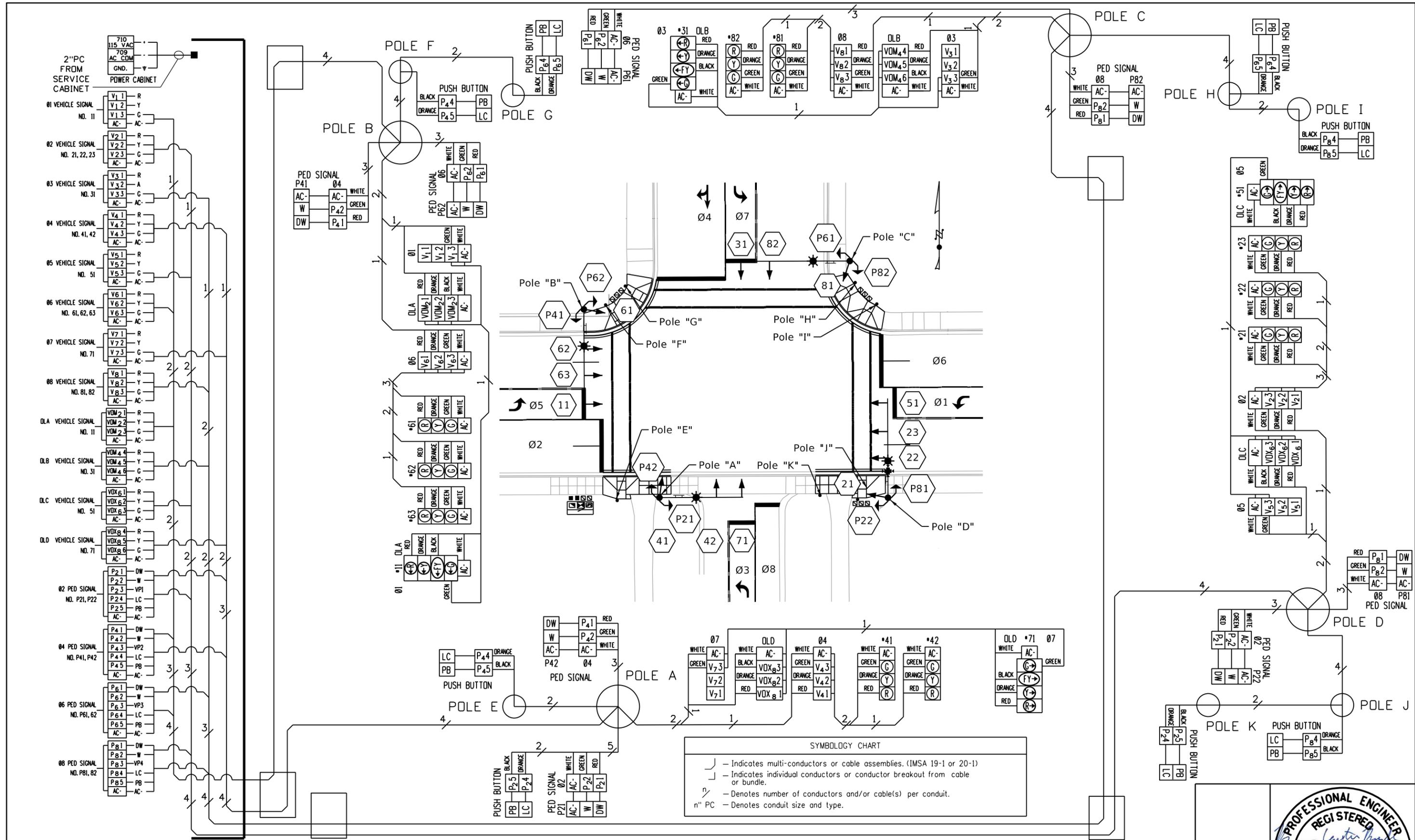
ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 17 OF 34





REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346 swir_001.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

PROJECT NO.
A024(346)

SIGNAL FIELD WIRE DIAGRAM

SMA-7866, INT 7TH SOUTH & CENTER ST
SIGNAL, REXBURG

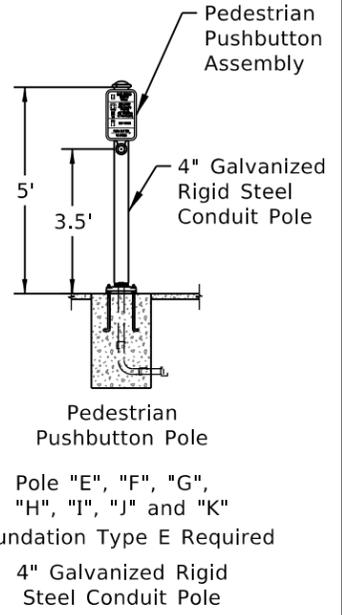
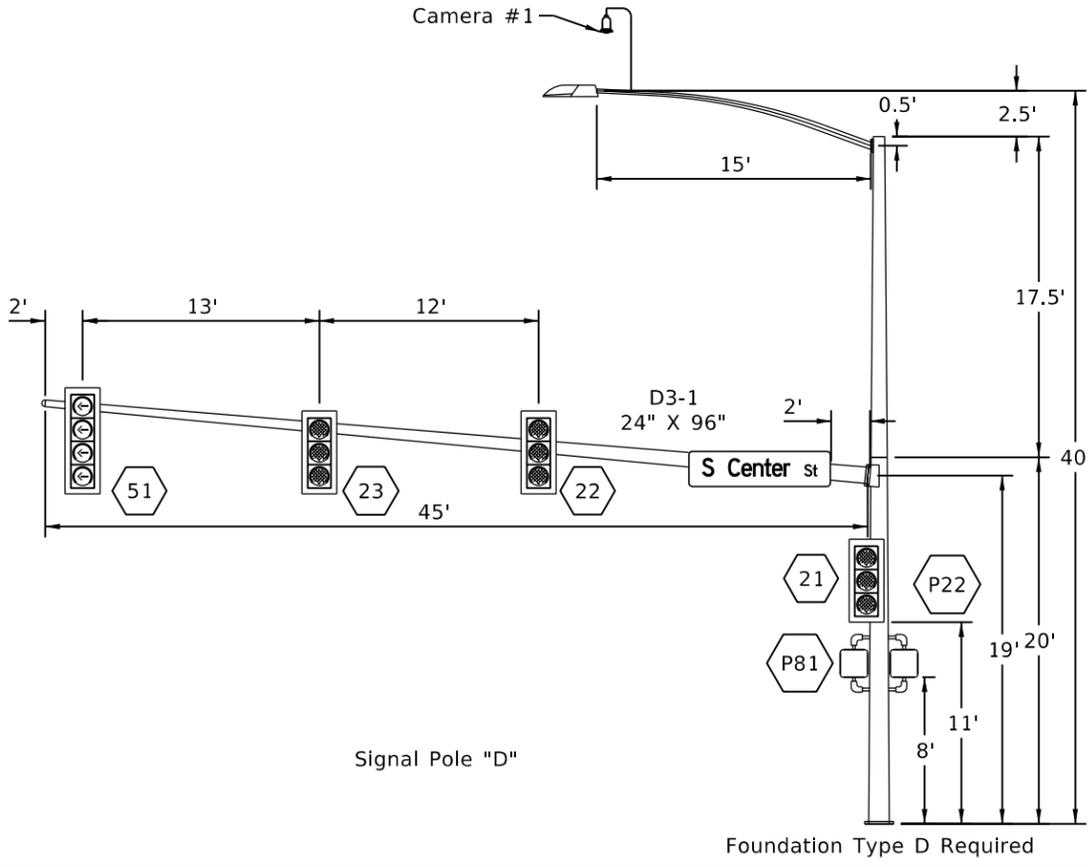
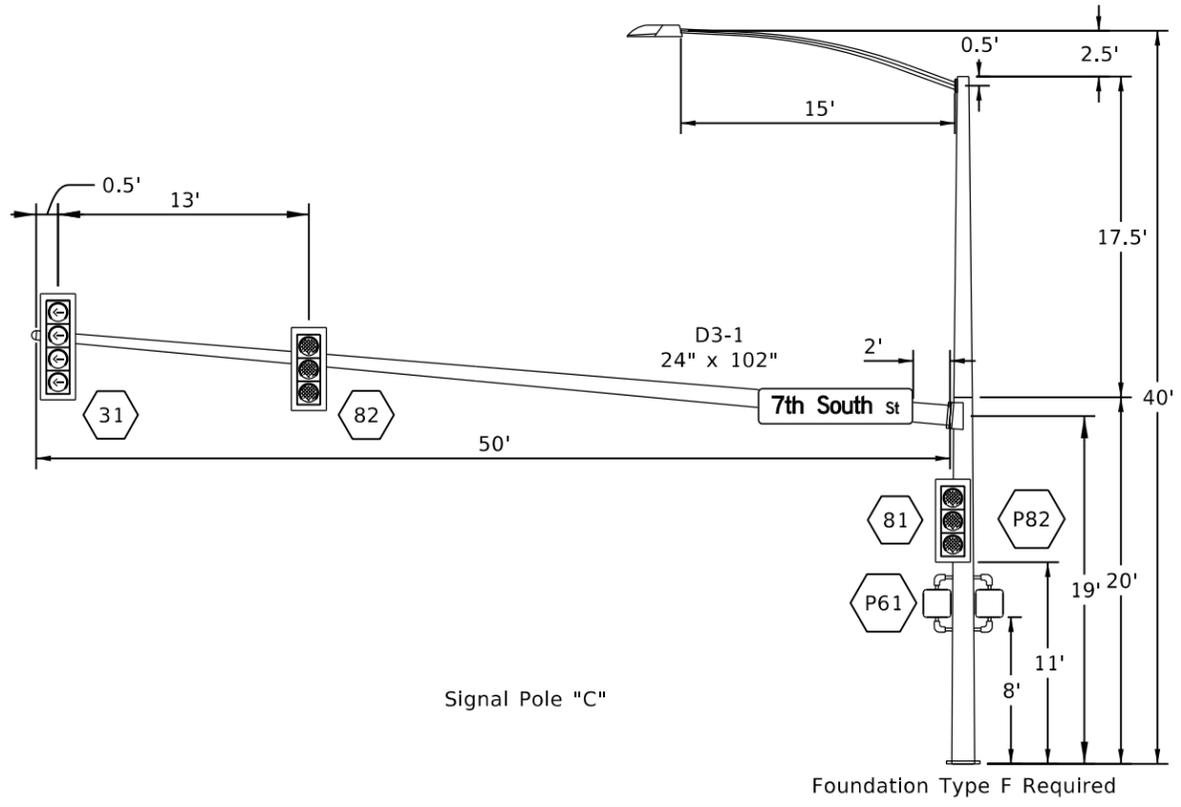
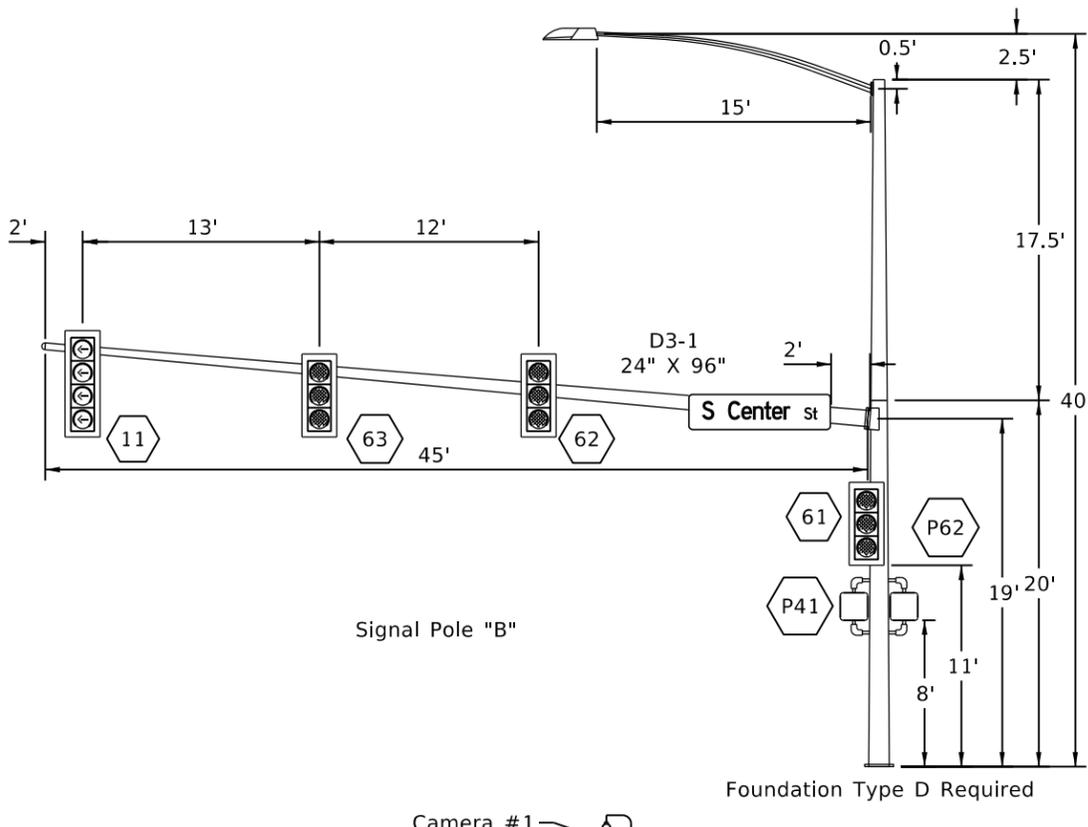
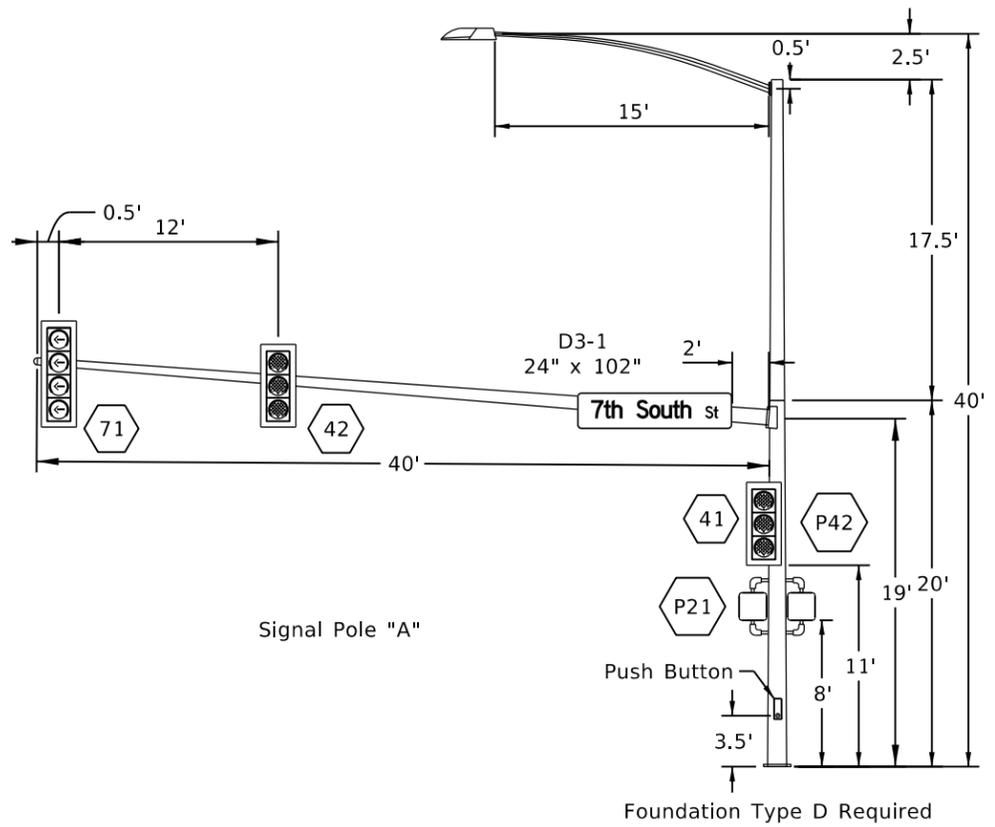
ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 18 OF 34





REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346_pole_001.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

PROJECT NO.
A024(346)

SIGNAL POLE MOUNTING DETAILS

SMA-7866, INT 7TH SOUTH & CENTER ST
SIGNAL, REXBURG

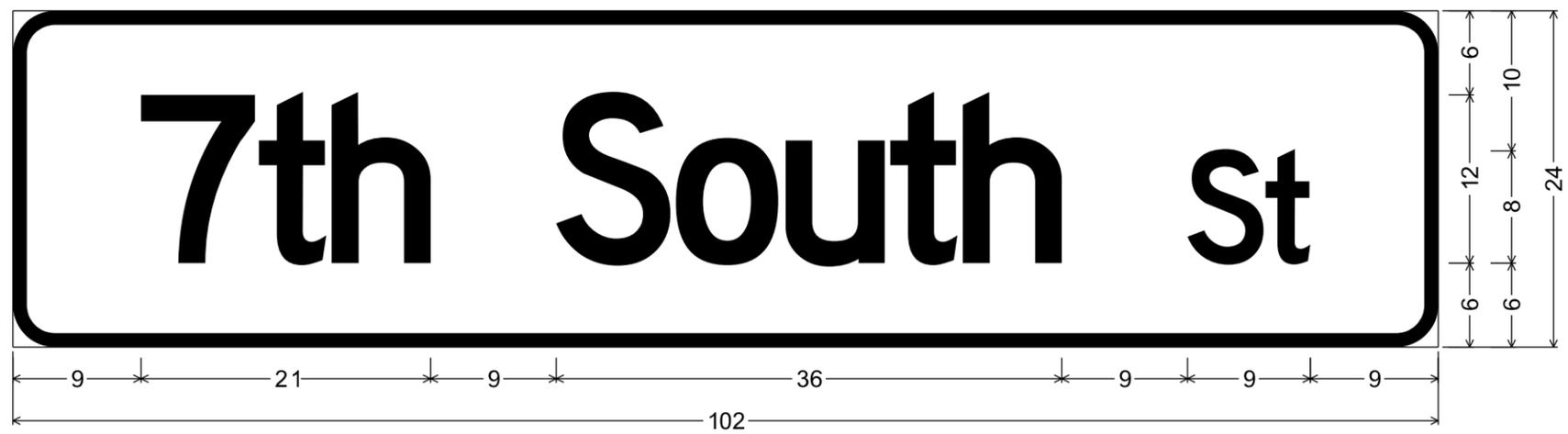
ENGLISH

COUNTY MADISON

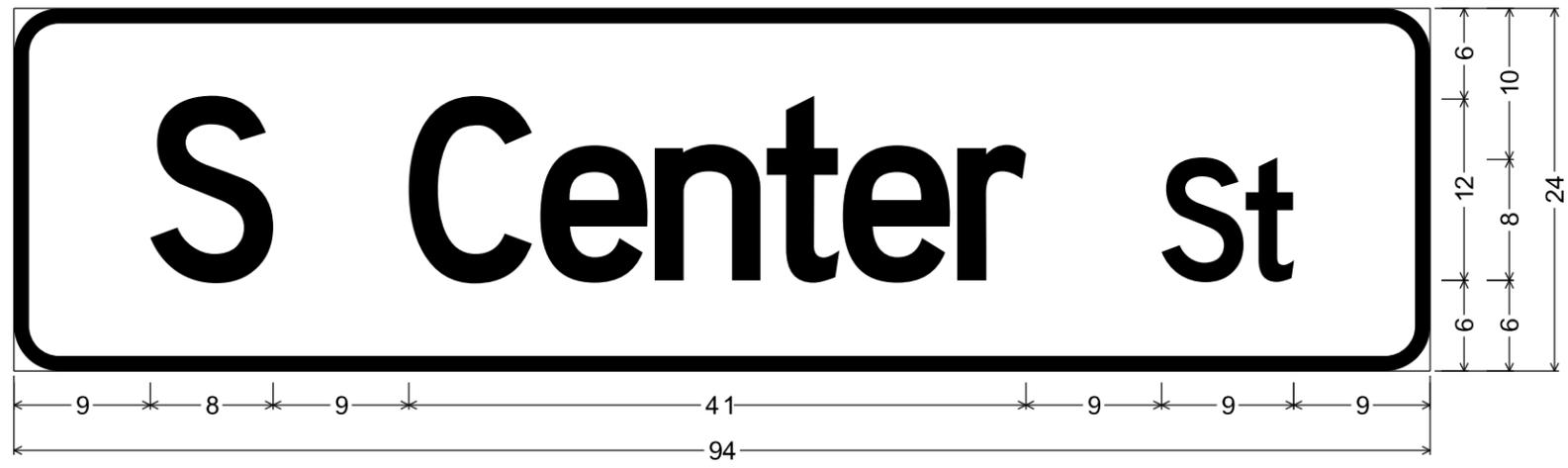
KEY NUMBER 24346

SHEET 19 OF 34

PROFESSIONAL ENGINEER
REGISTERED
12456
11/14/24
BENJAMIN CARTER BURKE
STATE OF IDAHO



3" Radius, 1" Border, White on Green;
 "7th", D 2K 30% spacing; "South", D 2K 30% spacing; "St", D 2K 30% spacing;



3" Radius, 1" Border, White on Green;
 "S", D 2K 30% spacing; "Center", D 2K 30% spacing; "St", D 2K 30% spacing;

SIGNAL HEAD SCHEDULE

	PEDESTRIAN
POLE A	
POLE B	
POLE C	
POLE D	

	12" RED 12" YELLOW 12" GREEN	12" RED ARROW 12" YELLOW ARROW 12" YELLOW ARROW 12" GREEN ARROW
POLE A		
POLE B		
POLE C		
POLE D		

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_pole_002.dgn
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

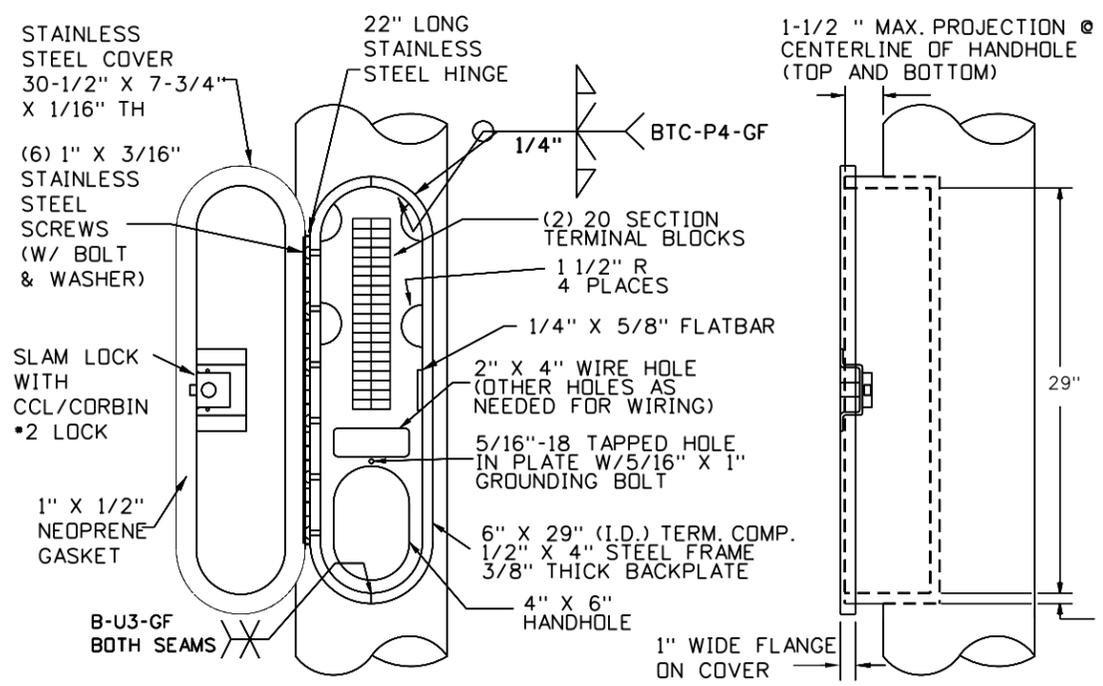
Horrocks.

PROJECT NO.
 A024(346)

TRAFFIC SIGNAL DETAILS
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

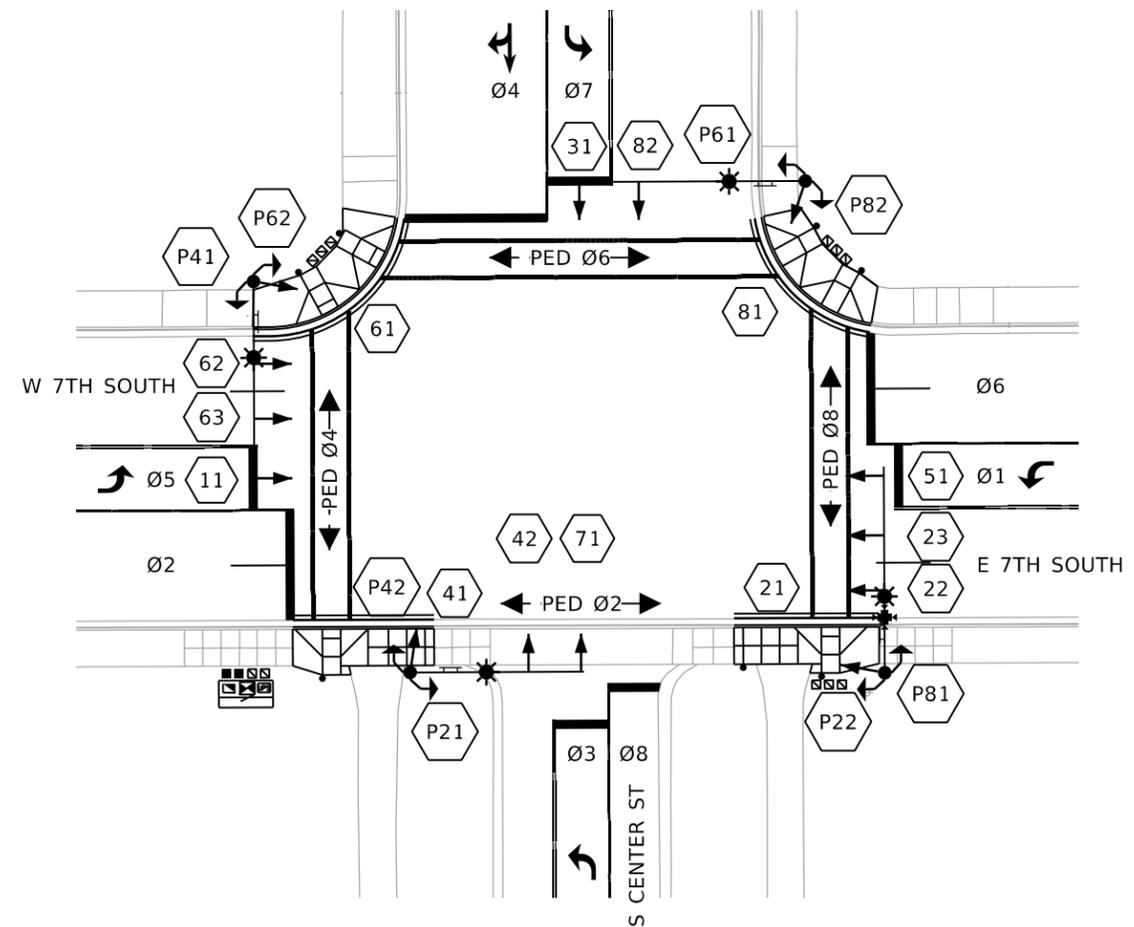
ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 20 OF 34



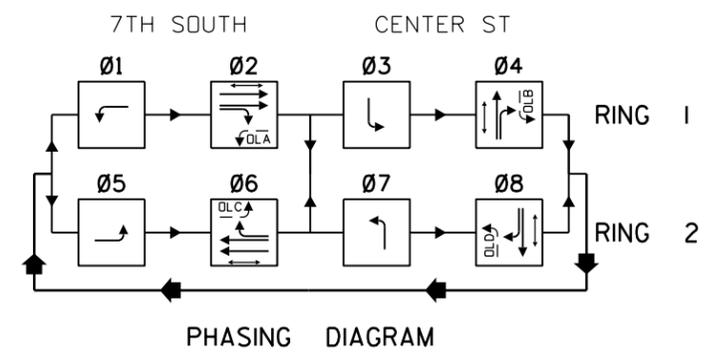


RECESSED TERMINAL COMPARTMENT DETAIL

PHASE	VEHICLE HEADS								PEDESTRIAN HEADS				CLEARANCE
	11	21-22-23	31	41-42	51	61-62-63	71	81-82	21-22	41-42	61-62	81-82	
Ø1	←G	R	←R	R	←R	R	←R	R	DW	DW	DW	DW	VEHICLE
Ø2	←Y	R	←R	R	←R	R	←R	R	DW	DW	DW	DW	
Ø3	←FY	G	←R	R	←R	R	←R	R	W	DW	DW	DW	PEDESTRIAN
	←Y	Y	←R	R	←R	R	←R	R	FDW	DW	DW	DW	
Ø4	←R	R	←G	R	←R	R	←R	R	DW	DW	DW	DW	VEHICLE
	←R	R	←Y	R	←R	R	←R	R	DW	DW	DW	DW	
Ø5	←R	R	←FY	G	←R	R	←R	R	DW	W	DW	DW	PEDESTRIAN
	←R	R	←FY	G	←R	R	←R	R	DW	FDW	DW	DW	
Ø6	←R	R	←Y	Y	←R	R	←R	R	DW	DW	DW	DW	VEHICLE
	←R	R	←R	R	←G	R	←R	R	DW	DW	DW	DW	
Ø7	←R	R	←R	R	←FY	G	←R	R	DW	DW	W	DW	PEDESTRIAN
	←R	R	←R	R	←FY	G	←R	R	DW	DW	FDW	DW	
Ø8	←R	R	←R	R	←Y	Y	←R	R	DW	DW	DW	DW	VEHICLE
	←R	R	←R	R	←R	R	←G	R	DW	DW	DW	DW	
FLASHING	←R	R	←R	R	←R	R	←FY	G	DW	DW	DW	W	PEDESTRIAN
	←R	R	←R	R	←R	R	←Y	Y	DW	DW	DW	DW	



SIGNAL SEQUENCE CHART



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 24346_phase_001.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

Horrocks.

PROJECT NO. A024(346)

TRAFFIC SIGNAL DETAILS

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 21 OF 34



SERVICE CABINET SCHEDULE

ITEM	CLASS "B"
① MAIN BREAKER	(SEE SCHEDULE)
② CONTROL BREAKER	1P-15A 120/240 VOLT
③ TEST SWITCH	SPST-15A 120/240 volt
④ LIGHTING BRANCH BREAKER	2P-120/240 VOLT (SEE DETAIL)
⑤ SIGNAL CONTROL BREAKER	1P-120/240 VOLT (SEE SCHEDULE)
⑥ NEMA 14-50R	120/240 VOLT
⑦ LIGHTING CONTACTOR	8P (ANY COMBINATION)
⑧ FLASHER CONTROL BREAKER	1P-120/240 VOLT (SEE SCHEDULE)
⑨ RECEPTACLE BRANCH BREAKER	2P-120/240 VOLT (SEE SCHEDULE)

BREAKER RATING SCHEDULE

CLASS "B"	
ITEM	NO. OF LIGHTING CIRCUITS
MAIN BREAKER	100 AMP 2P
LIGHTING BRANCH BREAKER	30 AMP 2P
SIGNAL CONTROL BREAKER	60 AMP 1P
LIGHTING CONTACTOR	30 AMP 4P
FLASHER CONTROL BREAKER	
SERVICE WIRE	NO. 3 THWN

NOTES:

- SERVICE CABINETS SHALL BE NEMA TYPE 3R. TO BE FURNISHED WITH A PADLOCK HASP.
- DEAD FRONT CONSTRUCTION IS REQUIRED ON ALL CABINETS.
- PLUG-IN TYPE BREAKERS SHALL BE USED ON "B" SERVICES. MAIN BREAKERS SHALL BE BOLT RETAINED.
- PRESSURE TYPE TERMINALS SHALL BE REQUIRED FOR #8 THROUGH #2 AWG. WIRE.
- ALL SERVICES SHALL BE U.L. LABELED FOR MAXIMUM 100 AMP BUS RATING, AND "APPROVED FOR SERVICE ENTRANCE EQUIPMENT."
- THE MAIN BREAKER LABEL SHALL BE MADE OF RED ON WHITE PLASTIC LAMINATE. THE BRANCH BREAKER LABELS SHALL BE MADE OF BLACK ON WHITE PLASTIC LAMINATE. THE LEGENDS SHALL BE ENGRAVED INTO THE STRIP SO AS TO PROVIDE WHITE LEGEND ON THE REQUIRED BACKGROUND. THE LABELS SHALL BE PERMANENTLY ATTACHED TO THE DEAD FRONT, NEXT TO THE CORRESPONDING BREAKERS.
- LIGHTING CONTACTORS, LIGHTING & SIGNAL BRANCH CIRCUIT BREAKERS TO BE FACTORY INSTALLED.
- BRANCH CIRCUIT WIRE AND PHOTOELECTRIC CONTROL TO BE SUPPLIED BY THE CONTRACTOR.
- ALL BUSSING TO BE 100 AMP RATED.
- WIRING SHALL BE THWN/MTW 600V 90°C RATED.

NOTE

IDENTIFICATION LABELS SHALL BE REQUIRED FOR BRANCH BREAKERS. LIGHTING BRANCH BREAKER LABELS SHALL READ: "LIGHTING CIRCUIT NO. (SEE PLANS)." SIGNAL BRANCH BREAKER LABELS SHALL READ: "SIGNAL AT (IDENTIFY TWO LEGS OF THE INTERSECTION)."

NOTE

WHEN SIGNAL & LIGHTING BRANCH BREAKERS ARE INSTALLED IN SAME PANEL BOARD, THE MAIN BREAKER SHALL BE PERMANENTLY LABELED AS FOLLOWS:

WARNING
DO NOT USE MAIN BREAKER AS A DISCONNECT
THIS PANEL BOARD CONTAINS ONE OR MORE BRANCH BREAKERS CONTROLLING A SIGNALIZED INTERSECTION

DRILL POLE CAP TO ACCEPT PHOTOELECTRIC CONTROL LOCKING TYPE RECEPTACLE. (SIGNAL POLES ONLY)

ILLUMINATION POLES USE POLE TOP ADAPTER FOR THIS LOCATION

ALTERNATE LOCATION

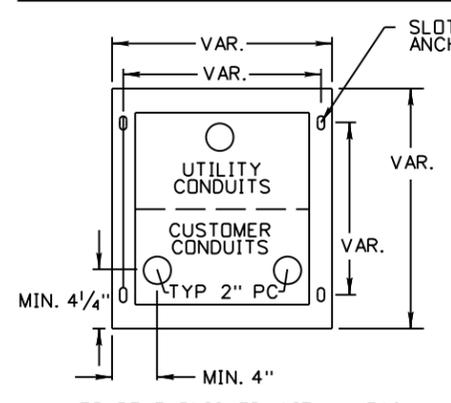
POLE TOP MOUNTING FOR PHOTOELECTRIC CELL

PHOTOELECTRIC CELL TWIST LOCK RECEPTACLE REQUIRED MOUNTED IN UPPER RIGHT SIDE OF CUSTOMER SECTION.

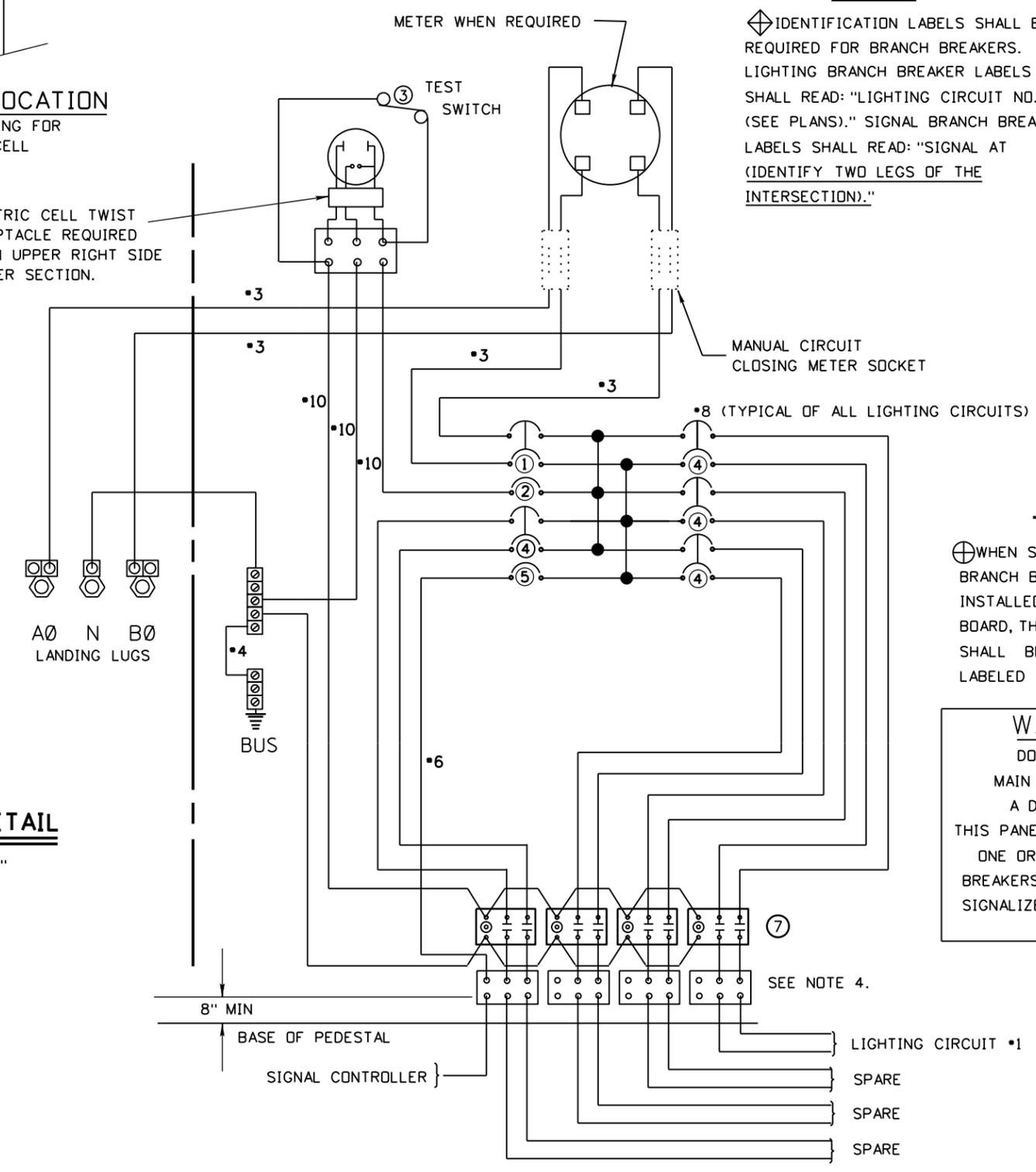
TYPICAL SERVICE PEDESTAL

SEE STANDARD DRAWING 1-7-A FOR FOUNDATION DETAIL

PEDESTAL FLANGE MOUNTING DETAIL



TO BE FURNISHED WITH 4 EACH 5/8" x 6" x 2"-13 NC ANCHOR BOLTS. NO SLEEVE REQUIRED.



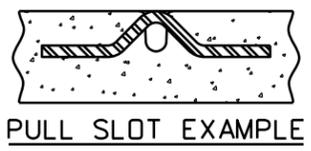
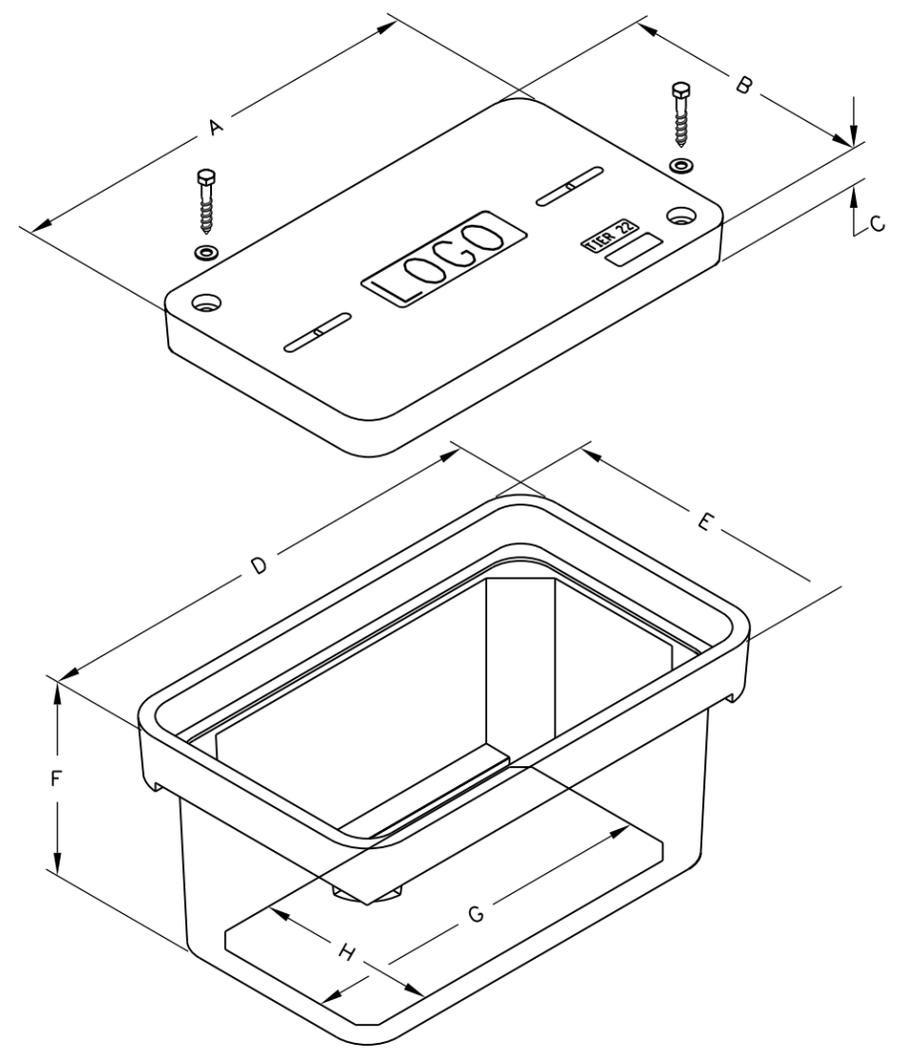
SHOW UNUSED LIGHTING CIRCUITS AS DASHED LINES AND LABEL AS "SPARE"

TYPE 1 SERVICE

REVISIONS				DESIGNED	SCALES SHOWN	PROJECT NO.	SERVICE PEDESTAL DETAIL	ENGLISH
NO.	DATE	BY	DESCRIPTION	R. TELFORD	ARE FOR 11" X 17" PRINTS ONLY	A024(346)	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG	COUNTY MADISON
				B. BURKE	CADD FILE NAME 24346 td01.dgn			KEY NUMBER 24346
				R. TELFORD	DRAWING DATE: November 14, 2024			SHEET 22 OF 34
				B. BURKE				

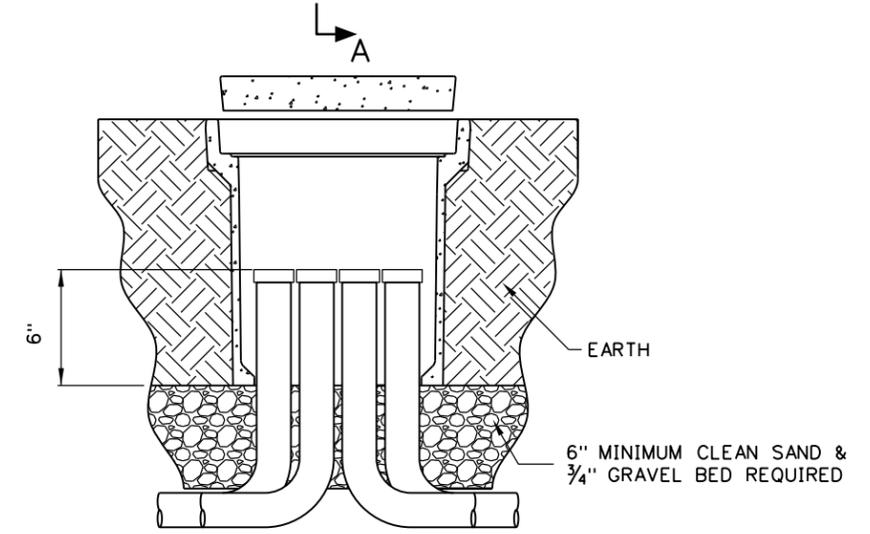
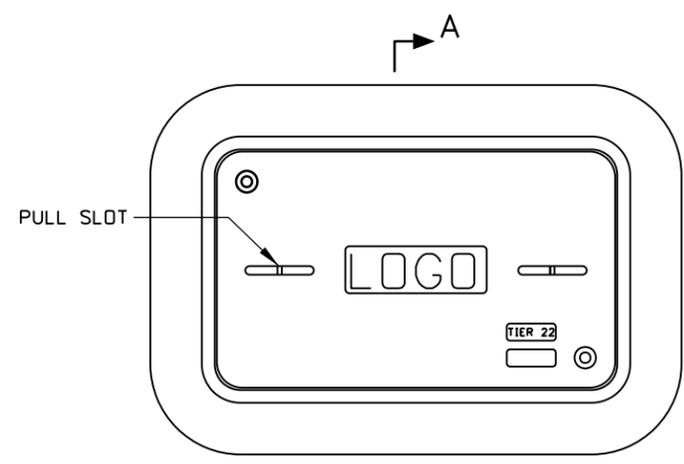
IDAHO TRANSPORTATION DEPARTMENT
YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

PROFESSIONAL ENGINEER
REGISTERED
12456
11/14/24
BENJAMN CARTER BURKE
STATE OF IDAHO



LOGO OPTIONS

- 1. BLANK
- 2. SIGNAL
- 3. LIGHTING



SECTION A-A
TYPICAL JUNCTION BOX

JUNCTION BOX NOTE:

POLYMER CONCRETE JUNCTION BOXES SHALL MEET THE REQUIREMENTS OF THE LATEST EDITION OF THE IDAHO TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, SUBSECTION 713.11D, COMPOSITE JUNCTION BOXES.

TABLE OF DIMENSIONS (NOMINAL)									
TYPE	DESCRIPTION	DIMENSIONS							
		A	B	C	D	E	F	G	H
PGB	JUNCTION BOX - (13" x 24" x 18")	23 ¹ / ₄ "	13 ³ / ₄ "	2"	25"	15 ¹ / ₂ "	18"	21 ¹ / ₄ "	11 ¹ / ₂ "
PGC	JUNCTION BOX - (17" x 30" x 18")	30 ¹ / ₂ "	17 ¹ / ₂ "	2"	32 ¹ / ₄ "	19 ¹ / ₄ "	18"	28 ¹ / ₂ "	15 ¹ / ₂ "
PGD	JUNCTION BOX - (24" x 36" x 24")	30 ⁵ / ₈ "	24"	3"	37 ⁵ / ₈ "	26"	24"	29 ³ / ₄ "	20 ³ / ₄ "

NOT TO SCALE

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_td12.dgn
 DRAWING DATE: November 14, 2024

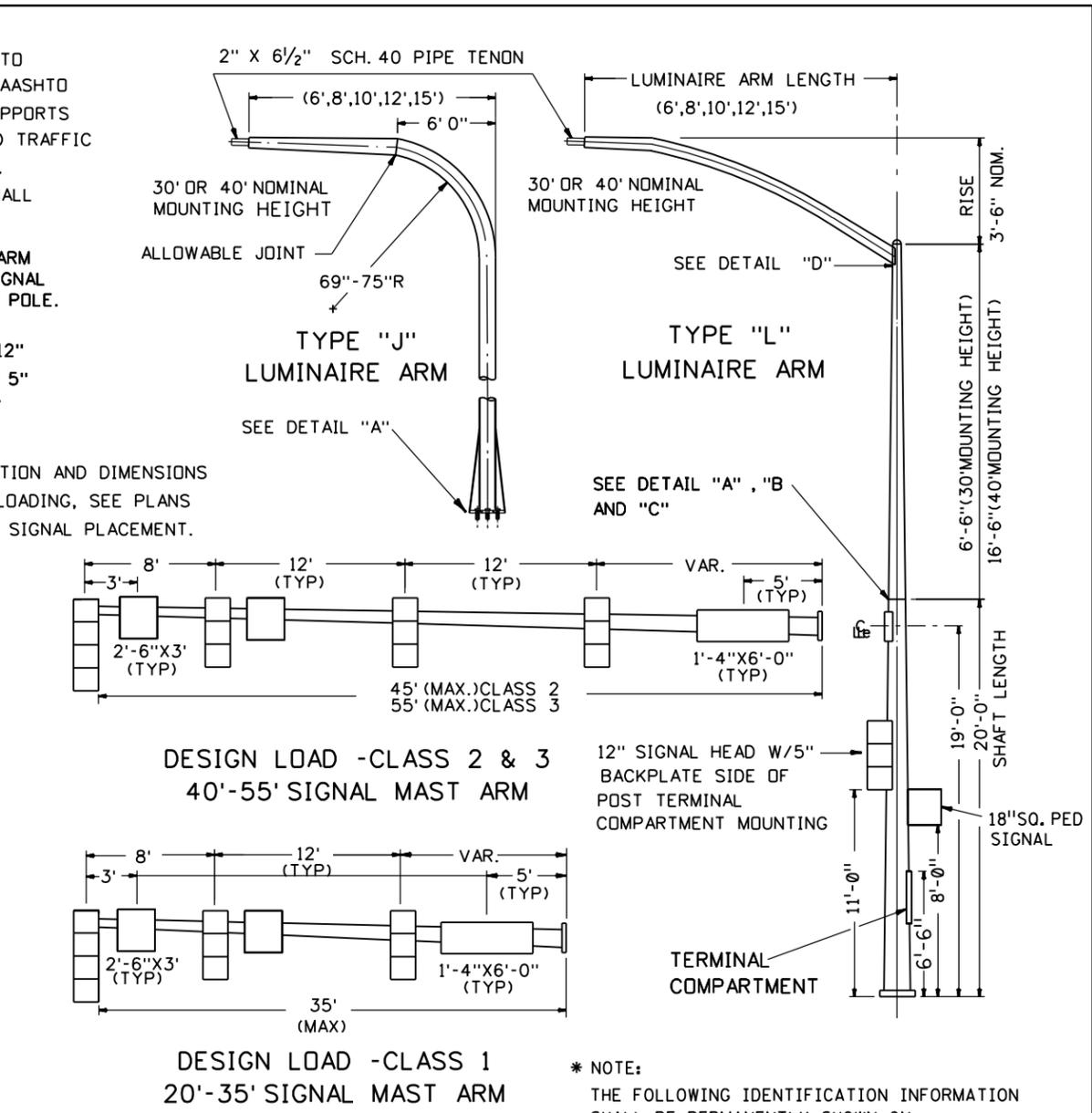
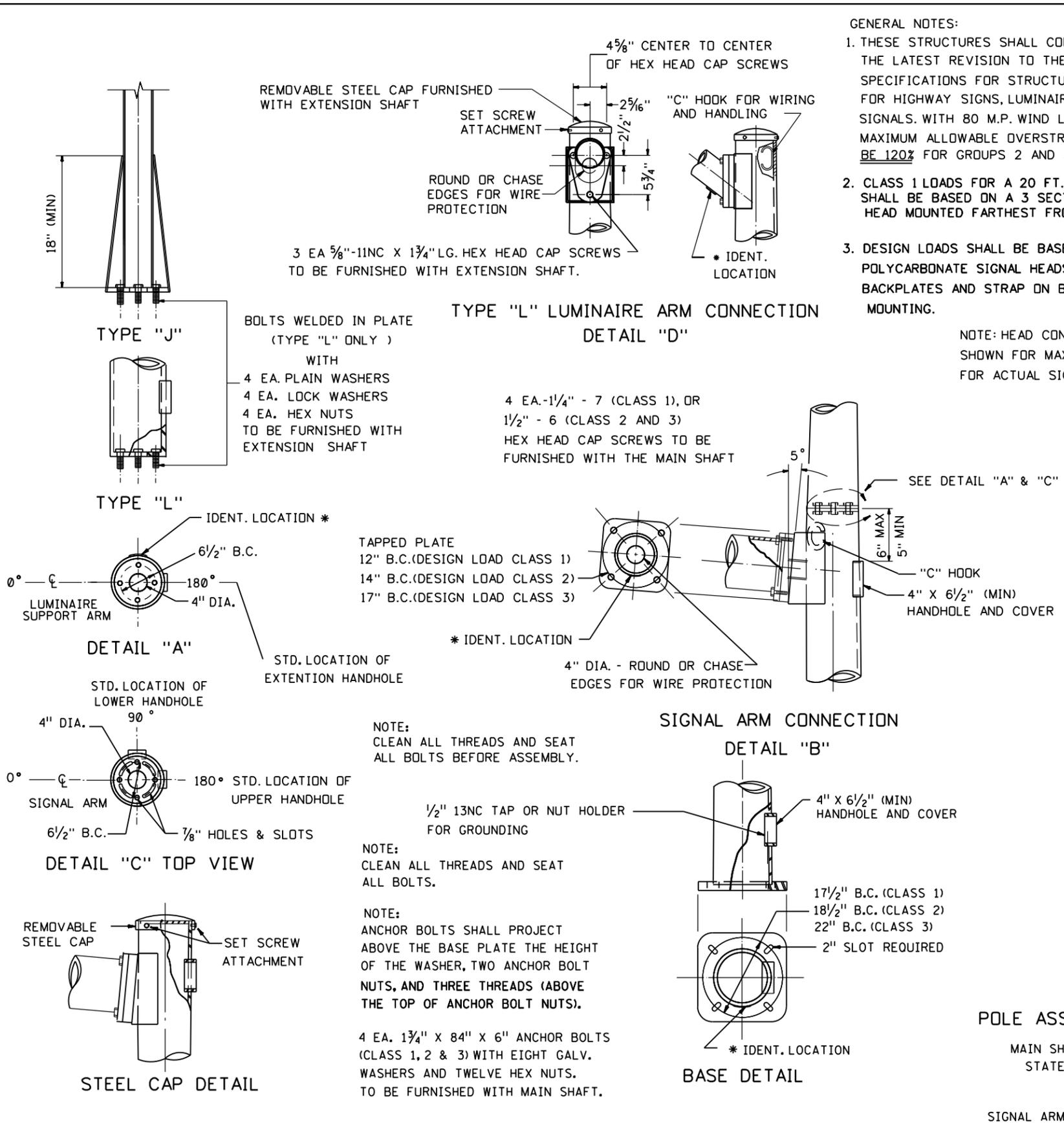
IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

Horrocks.

PROJECT NO.
A024(346)

JUNCTION BOX DETAIL
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 23 OF 34



SIGNAL POLE		LUMINAIRE ARM		
MAIN SHAFT	SIGNAL ARM LENGTH	ARM TYPE	MOUNTING HEIGHT	ARM LENGTH
CLASS 1	20'	J OR L	30'	6'
	25'			8'
	30'			10'
CLASS 2	35'		40'	12'
	40'			15'
CLASS 3	45'			
	50'			
	55'			

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 24346 td14.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety - YOUR Mobility - YOUR Economic Opportunity

Horrocks.

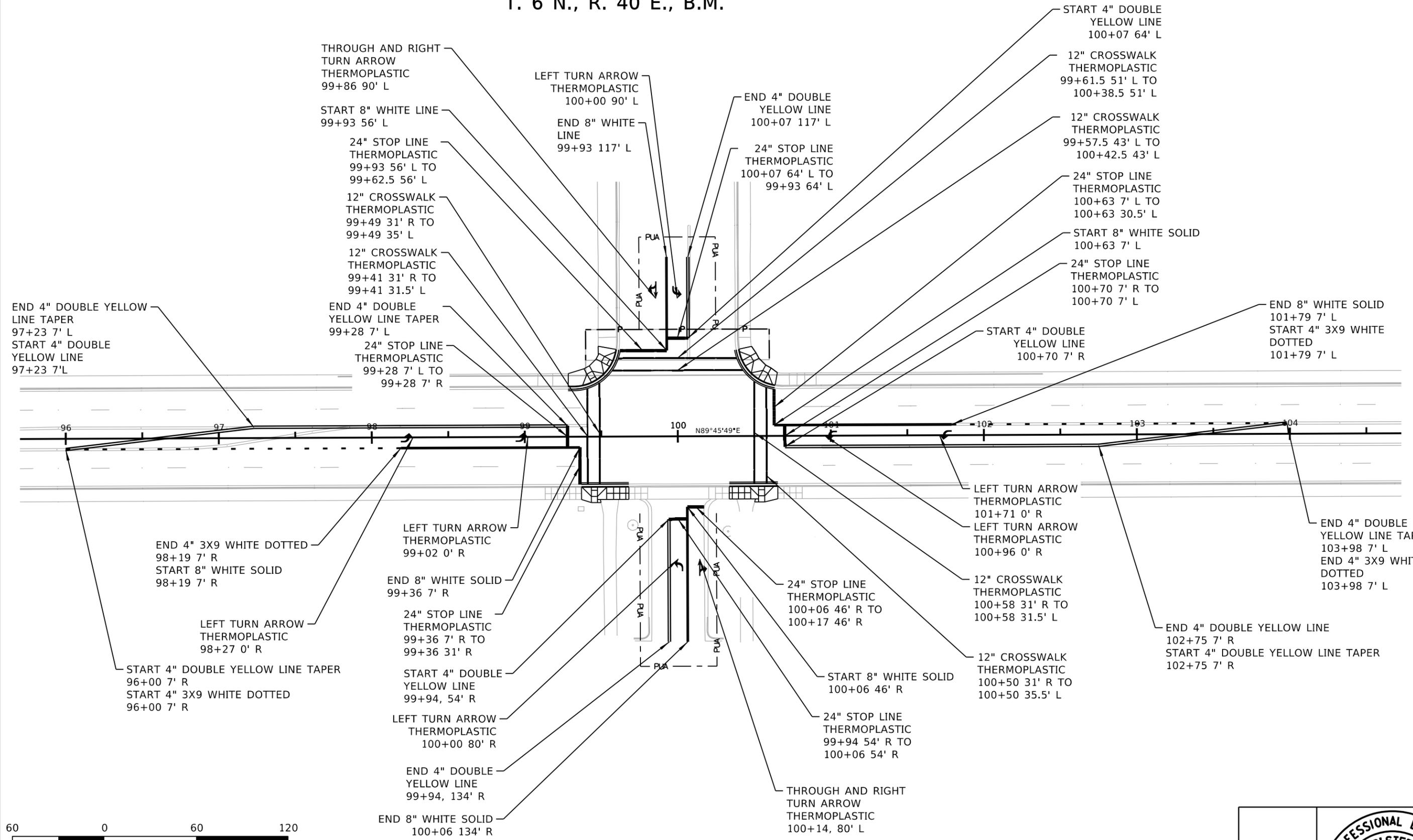
PROJECT NO.	A024(346)
-------------	-----------

SIGNAL POLE DETAIL	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
--------------------	---

ENGLISH	COUNTY: MADISON
KEY NUMBER	24346
SHEET	24 OF 34



T. 6 N., R. 40 E., B.M.



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY CADD FILE NAME 24346_sns_001.dgn DRAWING DATE: November 14, 2024
DESIGN CHECKED	B. BURKE	
DETAILED	R. TELFORD	
DRAWING CHECKED	B. BURKE	

IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

PROJECT NO.	A024(346)
-------------	-----------

STRIPING PLAN	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
---------------	---

ENGLISH	COUNTY MADISON
KEY NUMBER	24346
SHEET	25 OF 34



GENERAL NOTES:

1. ONCE THE SIGNAL IS INSTALLED, USE SIGNAL CONTROL BETWEEN 6:00 AM AND 10:00 PM. USE FLAGGER CONTROL WHEN THE SIGNAL IS NON-OPERATIONAL FROM FIRST INITIAL TURN-ON.
2. MOUNT ALL STAND MOUNTED SIGNS 5' ABOVE THE PAVED SURFACE. MOUNT ALL OTHER SIGNS 7' ABOVE THE PAVED SURFACE.
3. FURNISH, INSTALL AND MAINTAIN TWO WARNING FLAGS ON ALL TEMPORARY TRAFFIC CONTROL SIGNS. PAID UNDER MISC TEMPORARY TRAFFIC CONTROL ITEMS.
4. MEET OR EXCEED THE REQUIREMENTS OF THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS ADOPTED BY THE STATE FOR ALL TEMPORARY TRAFFIC CONTROL SIGNING.
5. REMOVE OR COVER ALL EXISTING WARNING, AND REGULATORY SIGNS IN CONFLICT WITH TEMPORARY TRAFFIC CONTROL SIGNING USING AN APPROVED METHOD WHILE TEMPORARY TRAFFIC CONTROL SIGNS ARE IN EFFECT.
6. THE DISTANCES SHOWN BETWEEN TRAFFIC CONTROL DEVICES ARE MINIMUMS. SOME ADJUSTMENT MAY BE NECESSARY IN THE FIELD DEPENDING ON CONDITIONS ENCOUNTERED.
7. PLACE ALL TEMPORARY TRAFFIC CONTROL SIGNING, TEMPORARY PAVEMENT MARKINGS, DRUMS, AND TUBULAR MARKERS PRIOR TO ANY DIVERSION OF TRAFFIC WITHIN THE WORK ZONE.
8. REMOVE ALL TEMPORARY TRAFFIC CONTROL FROM PROJECT IMMEDIATELY AFTER COMPLETION.
9. RETAIN AND PROTECT ALL EXISTING SIGNS SHALL BE RETAINED AND PROTECTED, UNLESS OTHERWISE NOTED.
10. MOUNT ANY TEMPORARY TRAFFIC CONTROL SIGNS LEFT UP IN THE SAME LOCATION AND IN USE FOR THREE DAYS OR LONGER ON WOOD OR STEEL POST.

APPROXIMATE SIGNING QUANTITIES

SIGN	CLASS	LOGO	SIZE	QTY.	SQ. FT.	TOTAL SQ. FT.
G20-2	B	END ROAD WORK	36" X 18"	4	4.5	18
M4-9aR	B	PED AND BIKE DETOUR RIGHT	30" X 24"	2	5	10
M4-9aL	B	PED AND BIKE DETOUR LEFT	30" X 24"	2	5	10
R9-9	B	SIDEWALK CLOSED	24" X 12"	4	2	8
W3-4	B	BE PREPARED TO STOP	48" X 48"	4	16	64
W4-2L(o)	B	LEFT LANE ENDS	48" X 48"	2	16	32
W4-2R(o)	B	RIGHT LANE ENDS	48" X 48"	2	16	32
W20-1	B	ROAD WORK AHEAD	48" X 48"	4	16	64
W20-5L	B	LEFT LANE CLOSED AHEAD	48" X 48"	2	16	32
W20-5R	B	RIGHT LANE CLOSED AHEAD	48" X 48"	2	16	32
W20-7	B	FLAGMAN	48" X 48"	4	16	64
SUB TOTAL						366
25% CONTINGENT						92
TOTAL						458

APPROXIMATE TEMPORARY TRAFFIC CONTROL QUANTITIES

ITEM DESCRIPTION	UNIT	TOTAL
TEMPORARY TRAFFIC CONTROL SIGNS	SF	458
DRUMS	EACH	24
ARROW BOARD TYPE C	HR	720
MISC TEMPORARY TRAFFIC CONTROL ITEMS	EACH	VARIES
TRAFFIC CONTROL MAINTENANCE	HR	90
WEIGHTED BASE TUBULAR MARKERS	EACH	76
FLAGGER CONTROL	HR	48
TEMPORARY ADA CURB RAMP	EA	6
PEDESTRIAN SAFETY WALL	FT	720
MODIFIED TYPE 3 BARRICADE	EA	4

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD	SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
DESIGN CHECKED B. BURKE	
DETAILED R. TELFORD	CADD FILE NAME 24346_phase_001.dgn
DRAWING CHECKED B. BURKE	DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

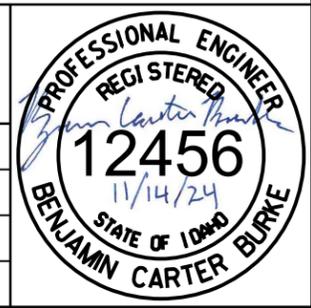


Horrocks.

PROJECT NO.	A024(346)
-------------	-----------

TEMPORARY TRAFFIC CONTROL	SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG
---------------------------	---

ENGLISH	COUNTY MADISON
KEY NUMBER	24346
SHEET	26 OF 34



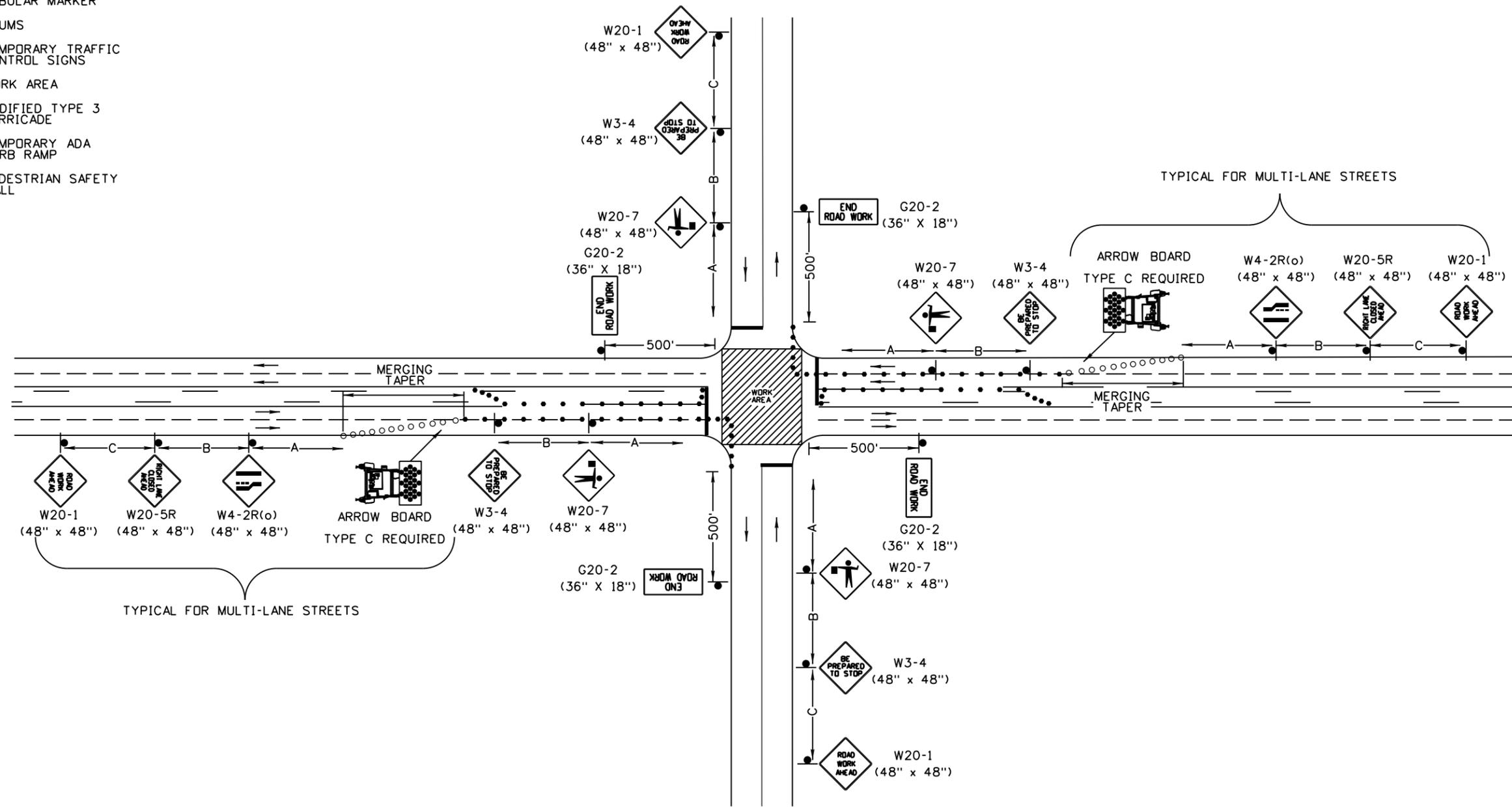
SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)

TYPICAL FLAGGING OPERATION

TYPICAL FOR ALL DIRECTIONS OF TRAFFIC
(NOT TO SCALE)

- WEIGHTED BASE TUBULAR MARKER
- DRUMS
- ⦿ TEMPORARY TRAFFIC CONTROL SIGNS
- ▨ WORK AREA
- ⊥ MODIFIED TYPE 3 BARRICADE
- ⊠ TEMPORARY ADA CURB RAMP
- ▩ PEDESTRIAN SAFETY WALL



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346_TTCP_002.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

PROJECT NO.
A024(346)

TEMPORARY TRAFFIC CONTROL

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

COUNTY **MADISON**

KEY NUMBER **24346**

SHEET **27** OF **34**

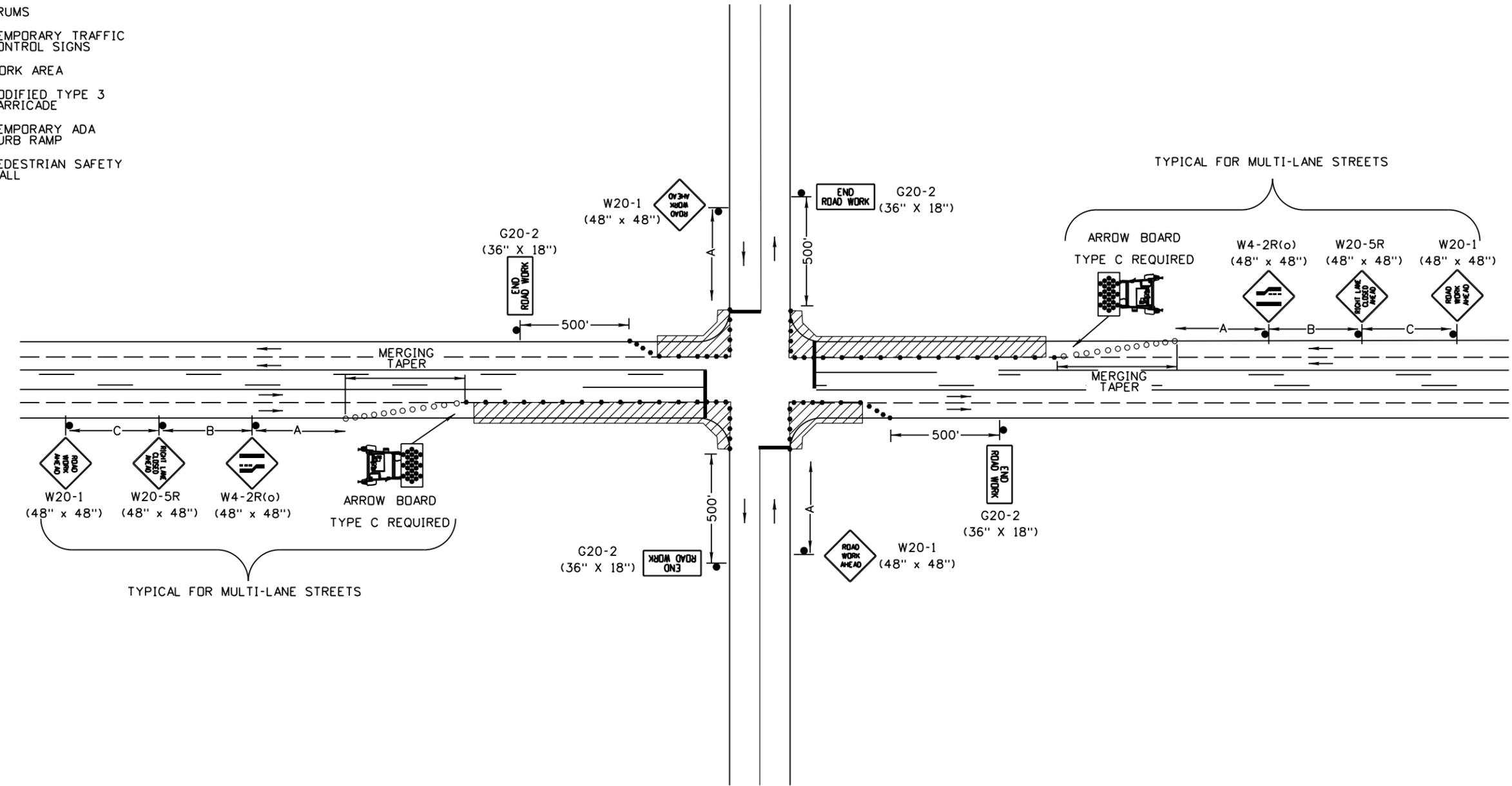
SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

TYPICAL OUTSIDE LANE CLOSURE OPERATION

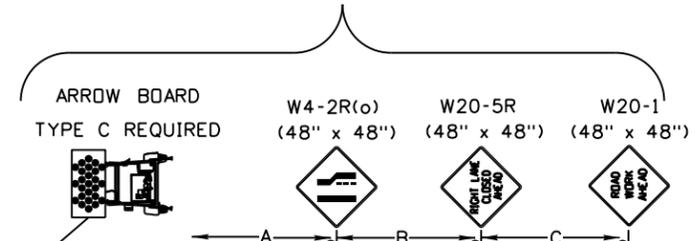
TYPICAL FOR ALL DIRECTIONS OF TRAFFIC (NOT TO SCALE)

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)

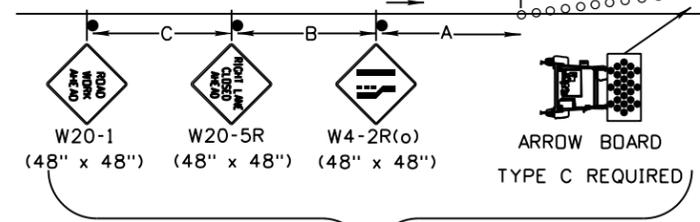
- WEIGHTED BASE TUBULAR MARKER
- DRUMS
- ⦿ TEMPORARY TRAFFIC CONTROL SIGNS
- ▨ WORK AREA
- ⊥ MODIFIED TYPE 3 BARRICADE
- ⊠ TEMPORARY ADA CURB RAMP
- ▩ PEDESTRIAN SAFETY WALL



TYPICAL FOR MULTI-LANE STREETS



TYPICAL FOR MULTI-LANE STREETS



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346_TTCP_003.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

Horrocks.

PROJECT NO.
A024(346)

TEMPORARY TRAFFIC CONTROL

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

COUNTY **MADISON**

KEY NUMBER **24346**

SHEET **28** OF **34**



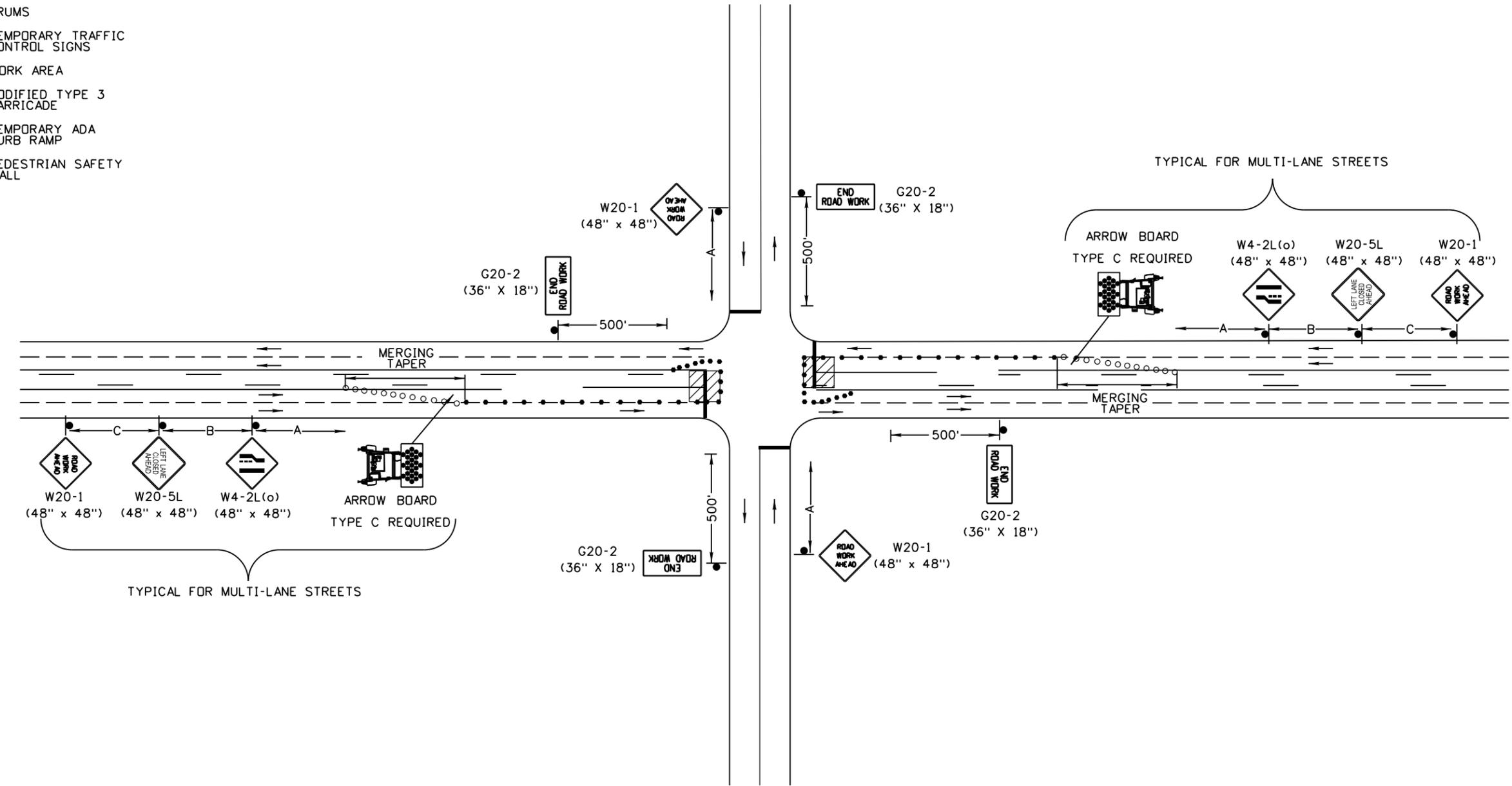
SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)

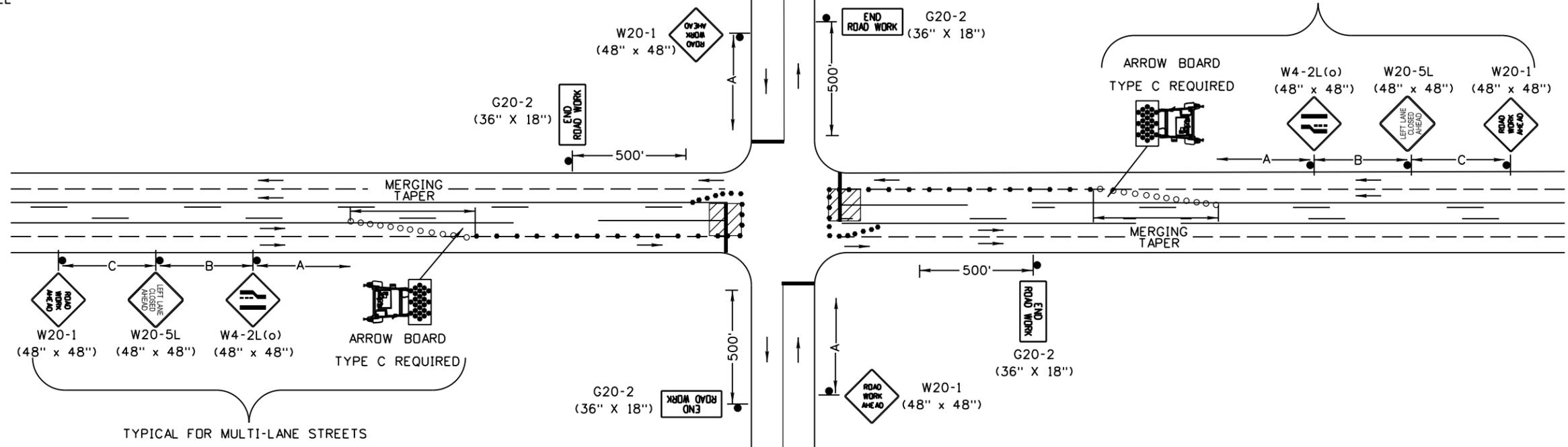
TYPICAL INSIDE LANE CLOSURE OPERATION

TYPICAL FOR ALL DIRECTIONS OF TRAFFIC (NOT TO SCALE)

- WEIGHTED BASE TUBULAR MARKER
- DRUMS
- ⦿ TEMPORARY TRAFFIC CONTROL SIGNS
- ▨ WORK AREA
- ⊥ MODIFIED TYPE 3 BARRICADE
- ⊠ TEMPORARY ADA CURB RAMP
- ▩ PEDESTRIAN SAFETY WALL



TYPICAL FOR MULTI-LANE STREETS



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED R. TELFORD
 DESIGN CHECKED B. BURKE
 DETAILED R. TELFORD
 DRAWING CHECKED B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME 24346_TTCP_004.dgn
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

PROJECT NO. A024(346)

TEMPORARY TRAFFIC CONTROL
 SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 29 OF 34

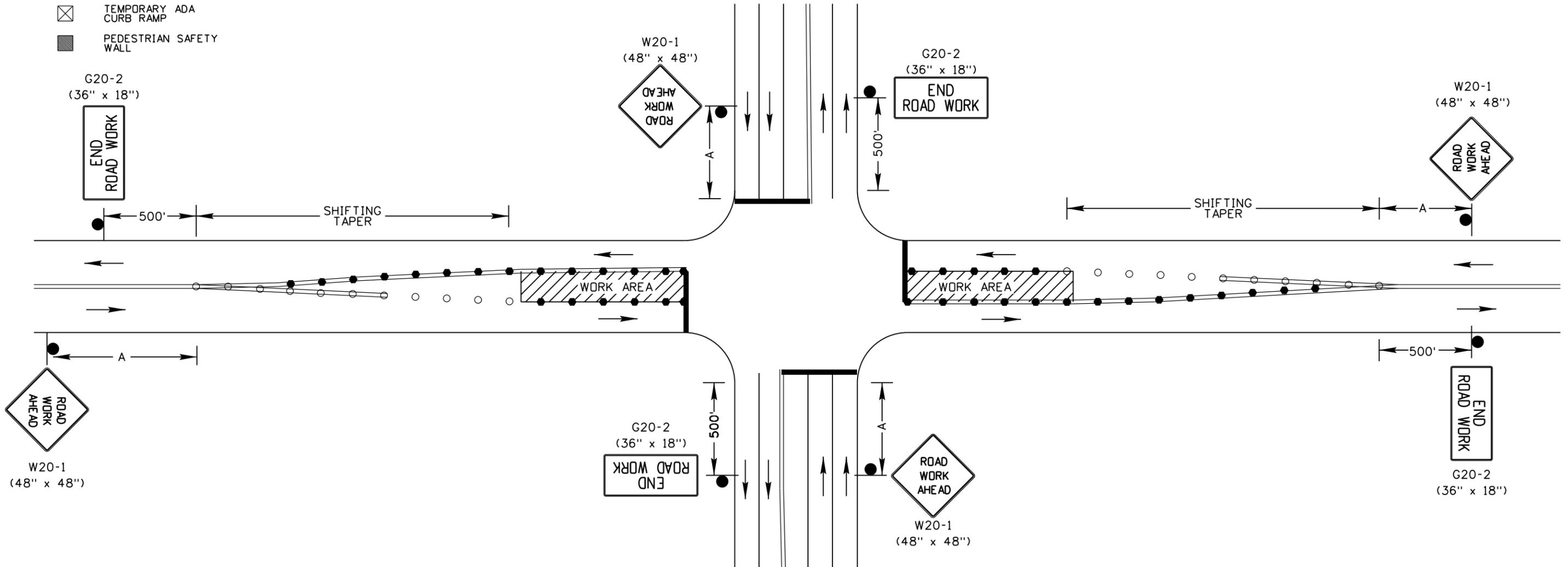
SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

TYPICAL LEFT TURN LANE CLOSURE OPERATION

TYPICAL FOR ALL DIRECTIONS OF TRAFFIC
(NOT TO SCALE)

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)

- WEIGHTED BASE TUBULAR MARKER
- DRUMS
- ▬ TEMPORARY TRAFFIC CONTROL SIGNS
- ▨ WORK AREA
- ▬ MODIFIED TYPE 3 BARRICADE
- ⊠ TEMPORARY ADA CURB RAMP
- PEDESTRIAN SAFETY WALL



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME
24346 TTCP 005.dgn

DRAWING DATE:
November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

PROJECT NO.
A024(346)

TEMPORARY TRAFFIC CONTROL

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

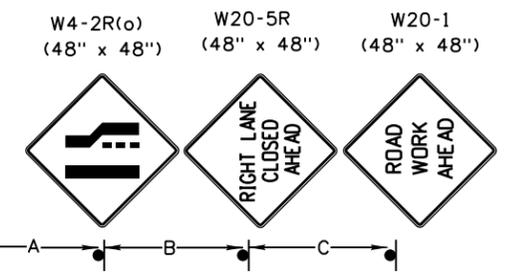
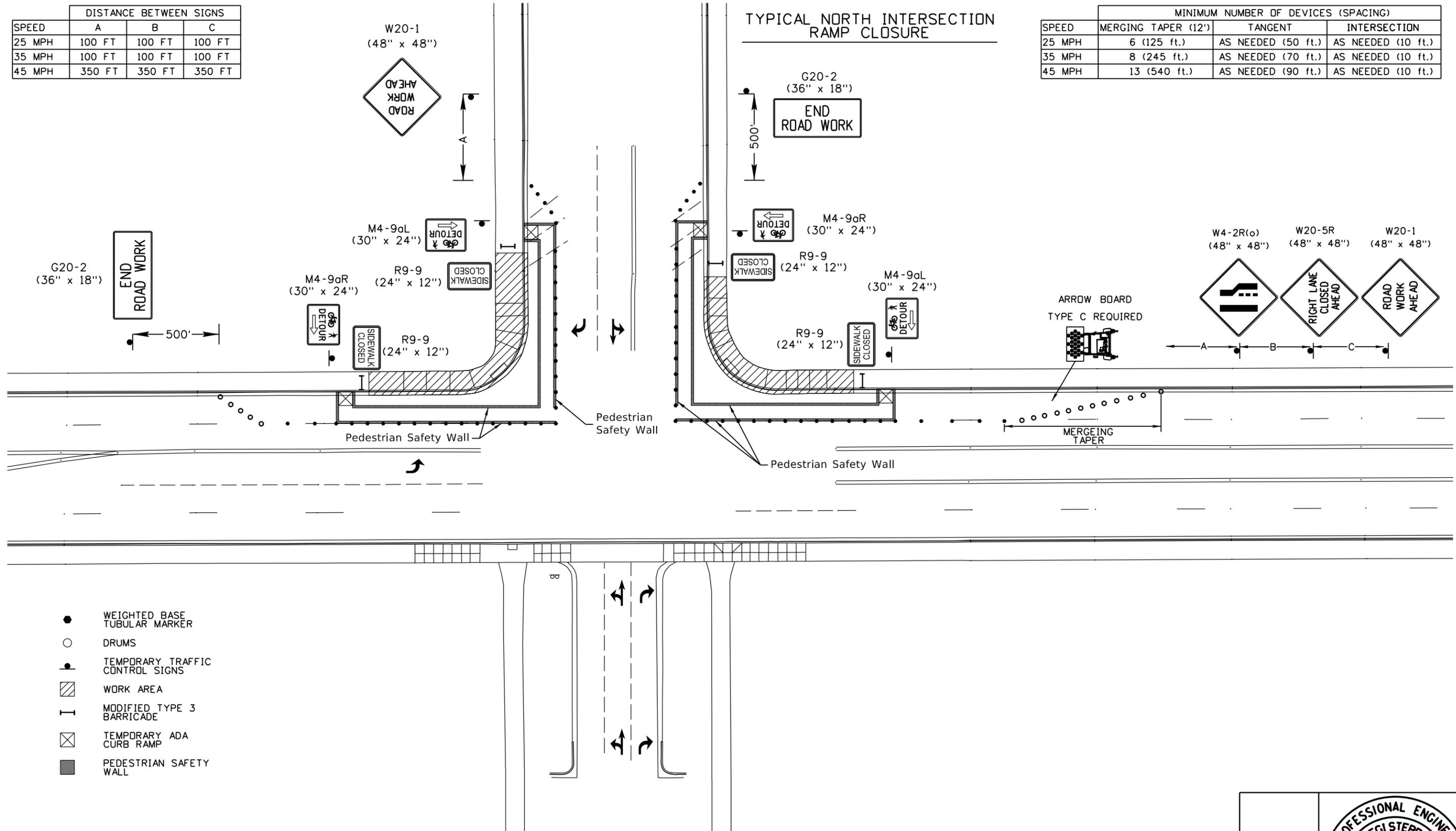
COUNTY MADISON

KEY NUMBER 24346

SHEET 30 OF 34

SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R.TELFORD
DESIGN CHECKED	B.BURKE
DETAILED	R.TELFORD
DRAWING CHECKED	B.BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 24346 TTCP 006.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

PROJECT NO. A024(346)

TEMPORARY TRAFFIC CONTROL

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 31 OF 34

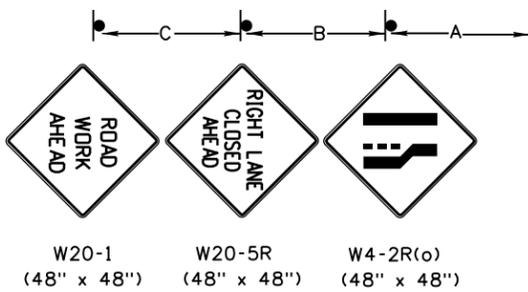
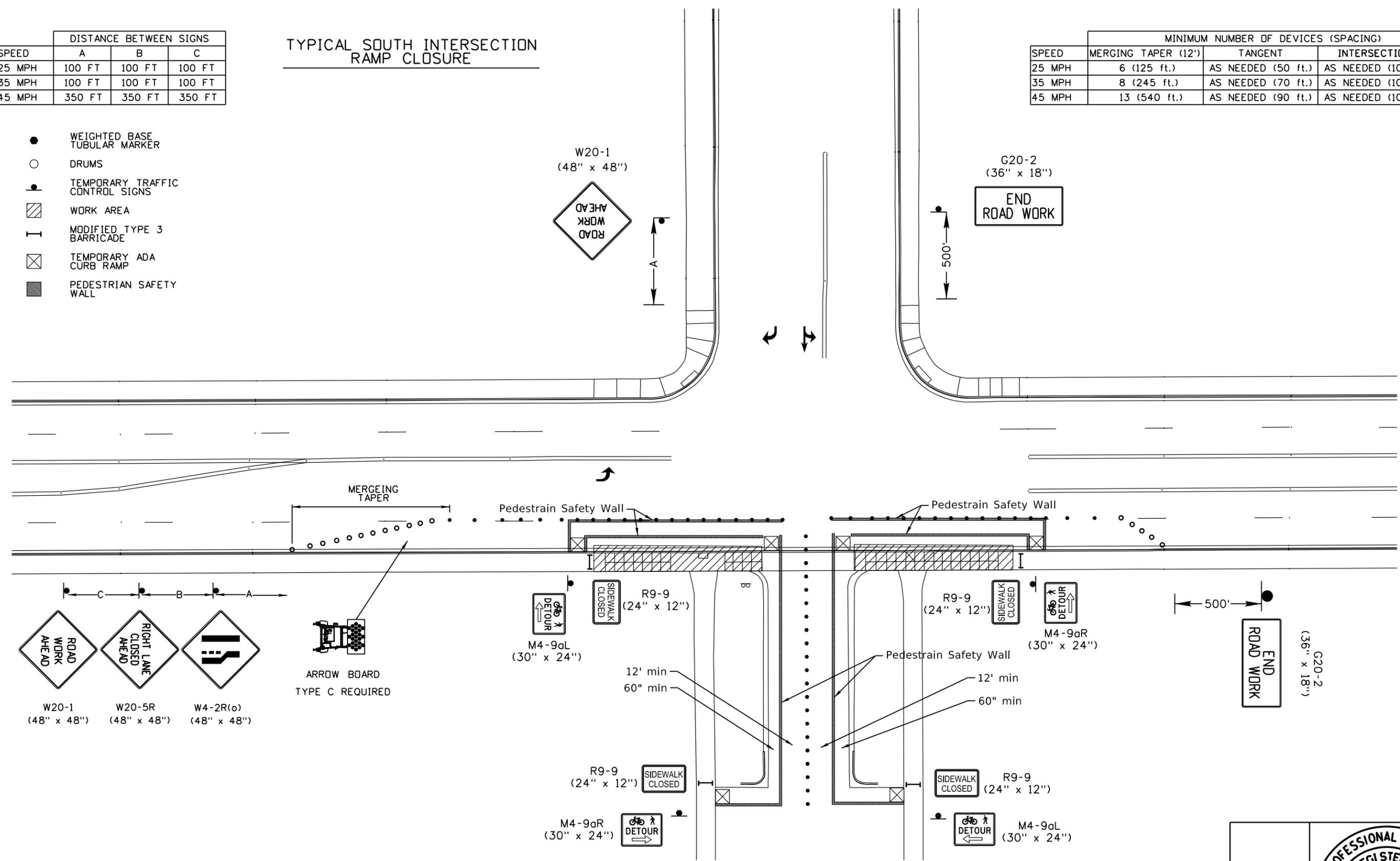


TYPICAL SOUTH INTERSECTION RAMP CLOSURE

SPEED	DISTANCE BETWEEN SIGNS		
	A	B	C
25 MPH	100 FT	100 FT	100 FT
35 MPH	100 FT	100 FT	100 FT
45 MPH	350 FT	350 FT	350 FT

SPEED	MINIMUM NUMBER OF DEVICES (SPACING)		
	MERGING TAPER (12')	TANGENT	INTERSECTION
25 MPH	6 (125 ft.)	AS NEEDED (50 ft.)	AS NEEDED (10 ft.)
35 MPH	8 (245 ft.)	AS NEEDED (70 ft.)	AS NEEDED (10 ft.)
45 MPH	13 (540 ft.)	AS NEEDED (90 ft.)	AS NEEDED (10 ft.)

- WEIGHTED BASE TUBULAR MARKER
- DRUMS
- ⦿ TEMPORARY TRAFFIC CONTROL SIGNS
- ▨ WORK AREA
- ⌋ MODIFIED TYPE 3 BARRICADE
- ⊠ TEMPORARY ADA CURB RAMP
- ▩ PEDESTRIAN SAFETY WALL



ARROW BOARD
 TYPE C REQUIRED

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 24346 TTCP.007.dgn
 DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT
 YOUR Safety-YOUR Mobility-YOUR Economic Opportunity

Horrocks.

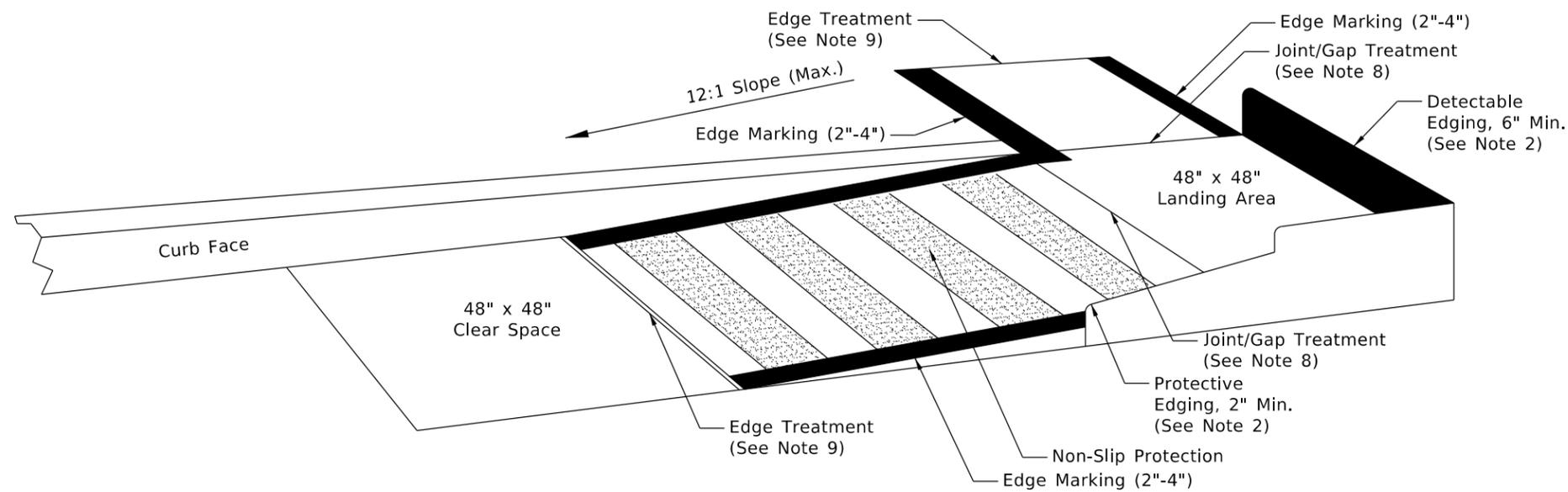
PROJECT NO.
 A024(346)

TEMPORARY TRAFFIC CONTROL
 SMA-7866, INT 7TH SOUTH & CENTER ST
 SIGNAL, REXBURG

ENGLISH
 COUNTY MADISON
 KEY NUMBER 24346
 SHEET 32 OF 34

PROFESSIONAL ENGINEER
 REGISTERED
 12456
 11/14/24
 BENJAMIN CARTER BURKE
 STATE OF IDAHO

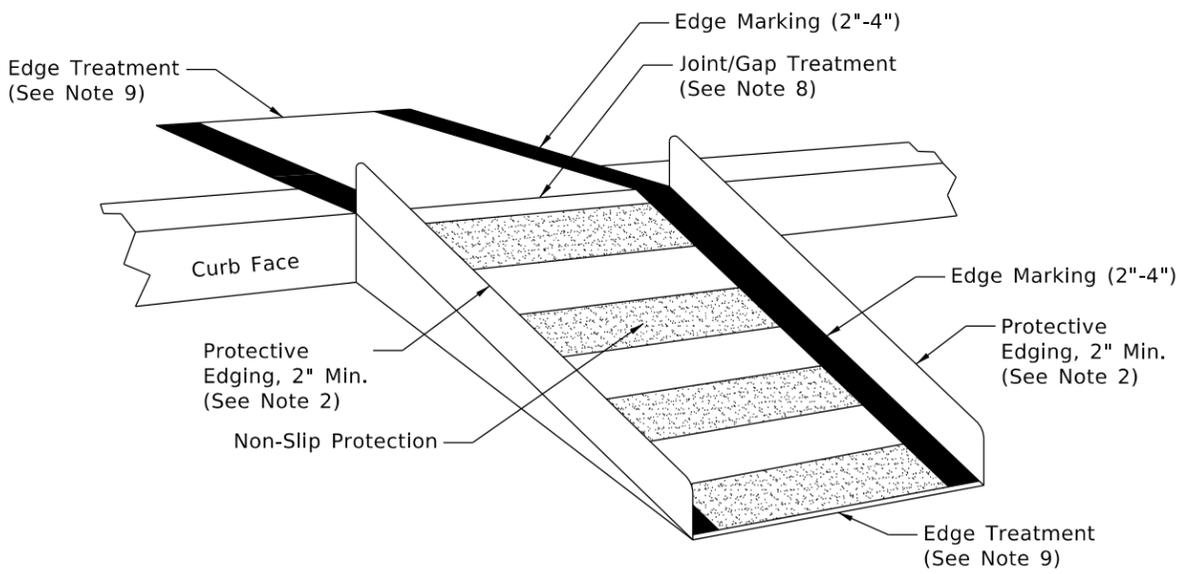
TEMPORARY ADA CURB RAMP (EXAMPLES)



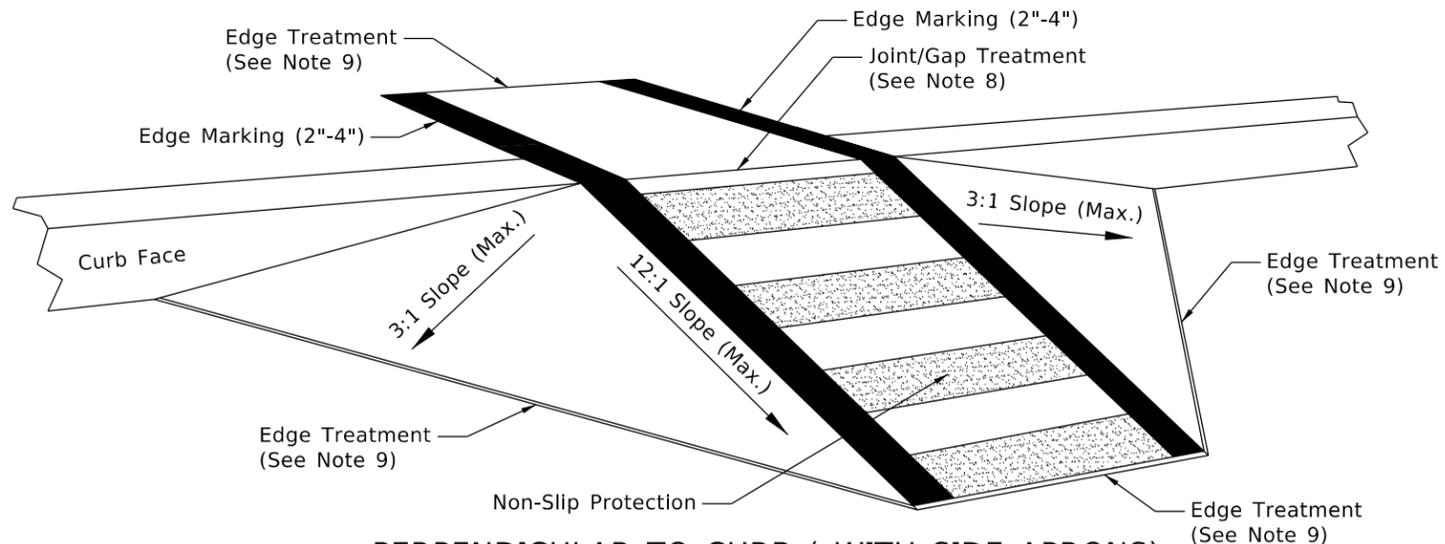
PARALLEL TO CURB

Notes:

1. Ramps shall be 48" minimum with a stable, non-slip surface.
2. Protective edging with a 2" minimum height shall be installed when the ramp or landing platform has a vertical drop of 6" or greater.
3. Detectable edging with a 6" minimum height and contrasting color shall be installed on all ramp landings where the walkway change direction.
4. Ramps and turning spaces shall have a 2% maximum cross slope.
5. Clear space of 48" x 48" minimum shall be provided above and below the ramp.
6. The ramp walkway edge shall be marked with a contrasting color 2" to 4" wide.
7. Water flow in the gutter (where applicable) shall not be restricted.
8. Lateral joints or gaps between surfaces shall be less than 0.5" wide.
9. Changes between surface heights shall not exceed 0.5". Lateral edges shall be vertical up to 0.25" high and beveled at 1:2 between 0.25" and 0.5" high.



PERPENDICULAR TO CURB (WITH PROTECTIVE EDGES)



PERPENDICULAR TO CURB (WITH SIDE APRONS)

REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 24346_TTCP_008.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety → YOUR Mobility → YOUR Economic Opportunity

PROJECT NO. A024(346)

TEMPORARY ADA CURB RAMP DETAIL

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

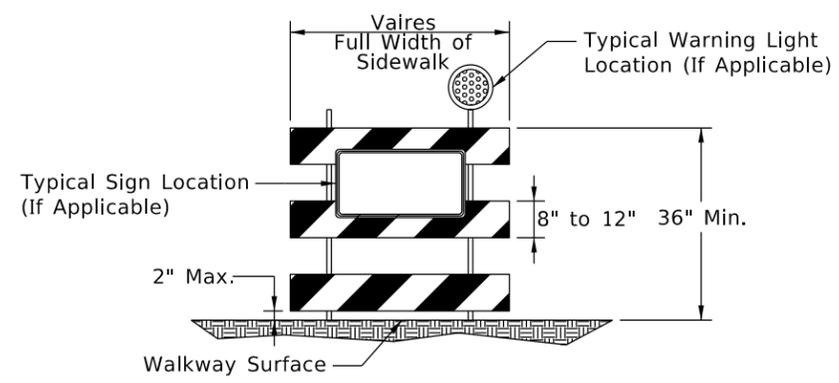
ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 33 OF 34

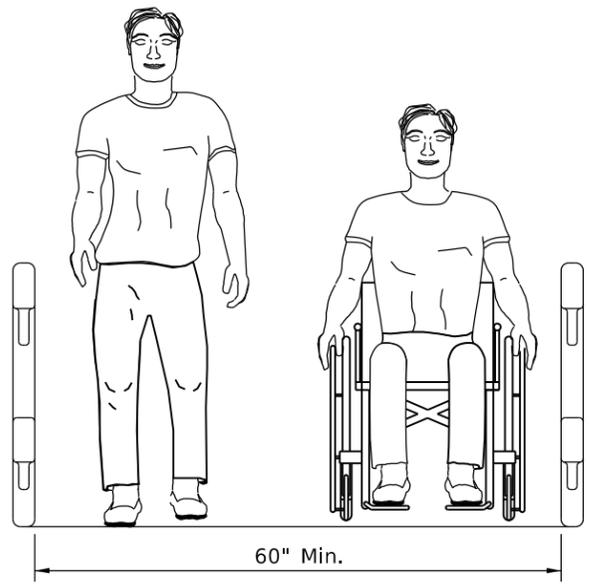
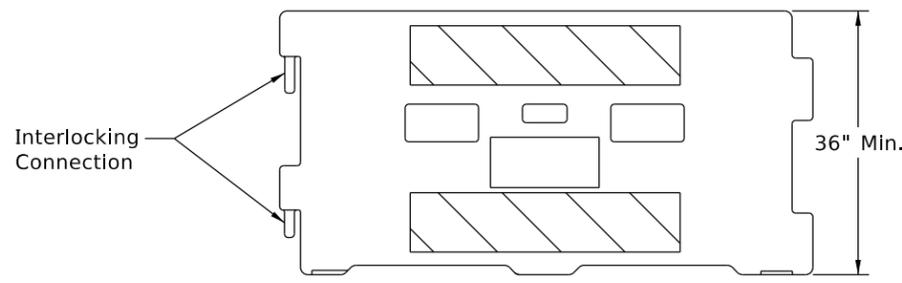
MODIFIED TYPE 3 BARRICADE



Notes:

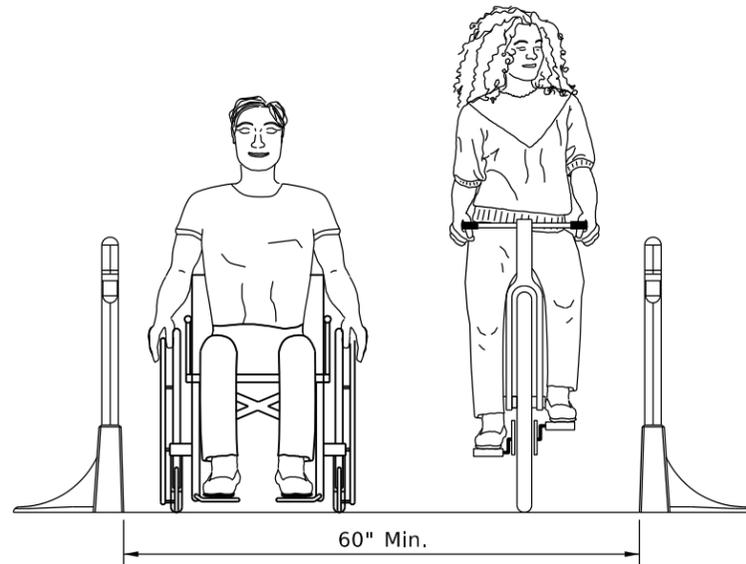
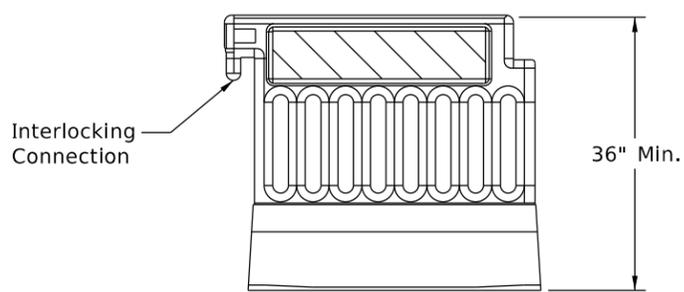
1. Any ballast required shall be located behind the barricade. Any support on the front of the safety wall shall not extend into the 60" minimum walkway clear space.
2. All barricades shall be free of sharp or rough edges. Fasteners (bolts) shall be rounded to prevent harm to hand, arms, or clothing.
3. Where barricades are used to close a sidewalk, the barricade shall extend across the full width of the sidewalk.
4. All barricades shall meet MASH Test Level 3.

PEDESTRIAN SAFETY WALL EXAMPLES



Notes:

1. Any ballast required shall be located behind the safety wall. Any support on the front of the safety wall shall not extend into the 60" minimum walkway clear space.
2. The safety wall shall have a continuous detectable edge no more than 2" above the walkway surface, and shall extend a minimum of 6" above the the walkway surface.
3. All safety walls shall have a color or markings that contrast with the walkway surface.
4. The safety wall shall not block or hinder water drainage from the roadway. Any gaps between the bottom of the safety wall and the walkway surface shall not be greater than 2".
5. The top of the safety wall shall: (1) be in a vertical plane perpendicular to the walkway; (2) be a continuous at a height of 36" to 38" above the walkway surface; (3) be supported with a minimal interference to a pedestrian's hand or fingers.
6. All safety wall products used to channelize pedestrians shall interlock and be free of sharp or rough edges to prevent harm to hand, arms, or clothing.
7. All safety wall products shall meet MASH Test Level 3.



REVISIONS			
NO.	DATE	BY	DESCRIPTION

DESIGNED	R. TELFORD
DESIGN CHECKED	B. BURKE
DETAILED	R. TELFORD
DRAWING CHECKED	B. BURKE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 24346_TTCP_009.dgn

DRAWING DATE: November 14, 2024

IDAHO TRANSPORTATION DEPARTMENT

YOUR Safety→YOUR Mobility→YOUR Economic Opportunity

Horrocks.

PROJECT NO. A024(346)

PEDESTRIAN DETOUR DETAILS

SMA-7866, INT 7TH SOUTH & CENTER ST SIGNAL, REXBURG

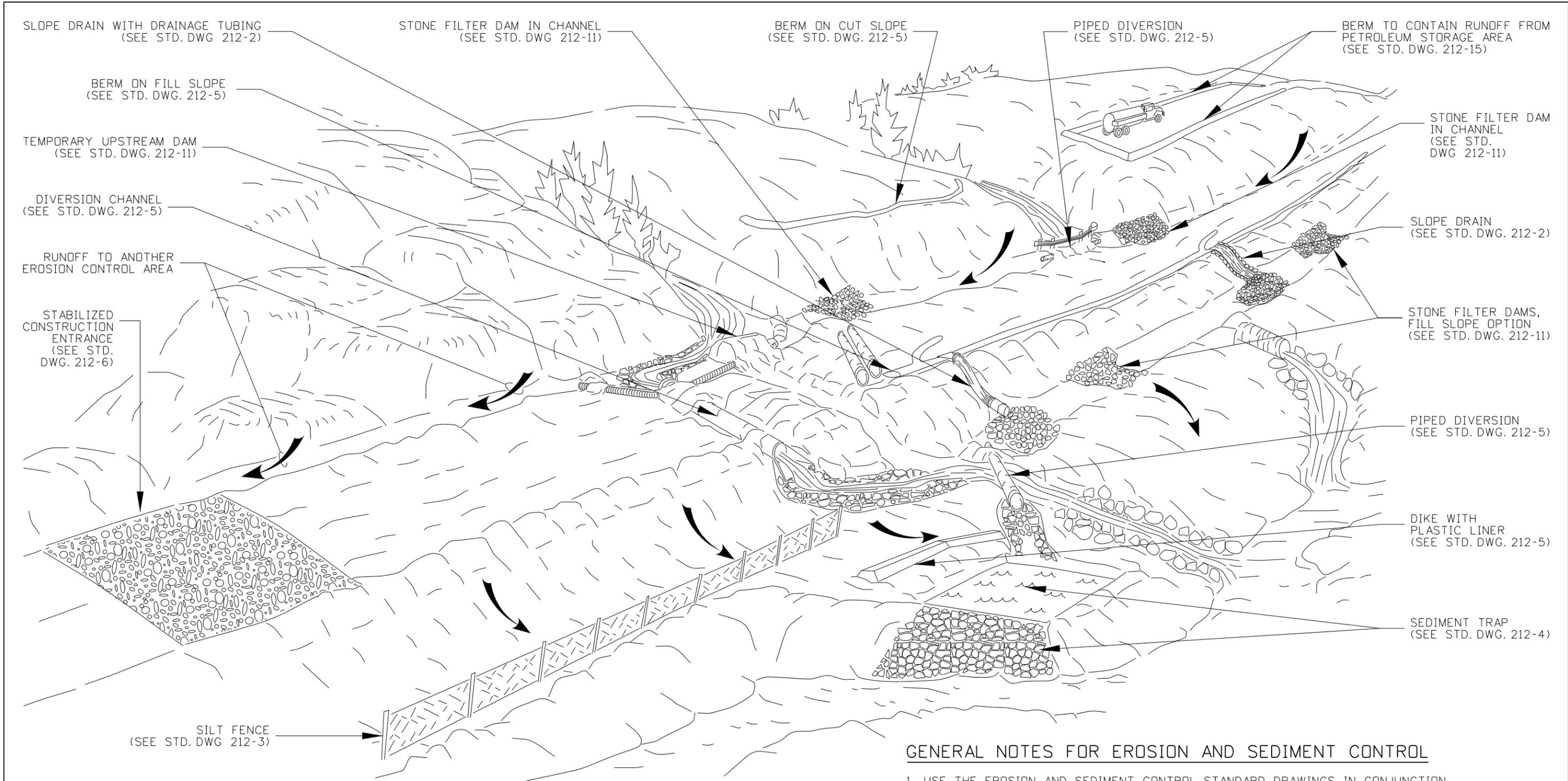
ENGLISH

COUNTY MADISON

KEY NUMBER 24346

SHEET 34 OF 34





GENERAL NOTES FOR EROSION AND SEDIMENT CONTROL

1. USE THE EROSION AND SEDIMENT CONTROL STANDARD DRAWINGS IN CONJUNCTION WITH THE ITD BEST MANAGEMENT PRACTICES MANUAL.
2. THE PLACEMENT OF EROSION CONTROL MEASURES IS SITE SPECIFIC. OBTAIN THE ENGINEER'S APPROVAL OF THE EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO INSTALLATION.
3. EROSION AND SEDIMENT CONTROL MEASURES PLACEMENT AND INSTALLATION MAY BE CONTROLLED BY THE NPDES, 404 PERMIT OR CONTRACT SPECIFICATIONS.
4. DRAWING NOT TO SCALE

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	9-93	MSM	6	12-16	RDL			
2	6-96	MSM	7	02-21	TWF			
3	10-10	KEH						
4	10-11	KEH						
5	12-12	RDL						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 212-01_0421.dgn
 DRAWING DATE: APRIL, 1993

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

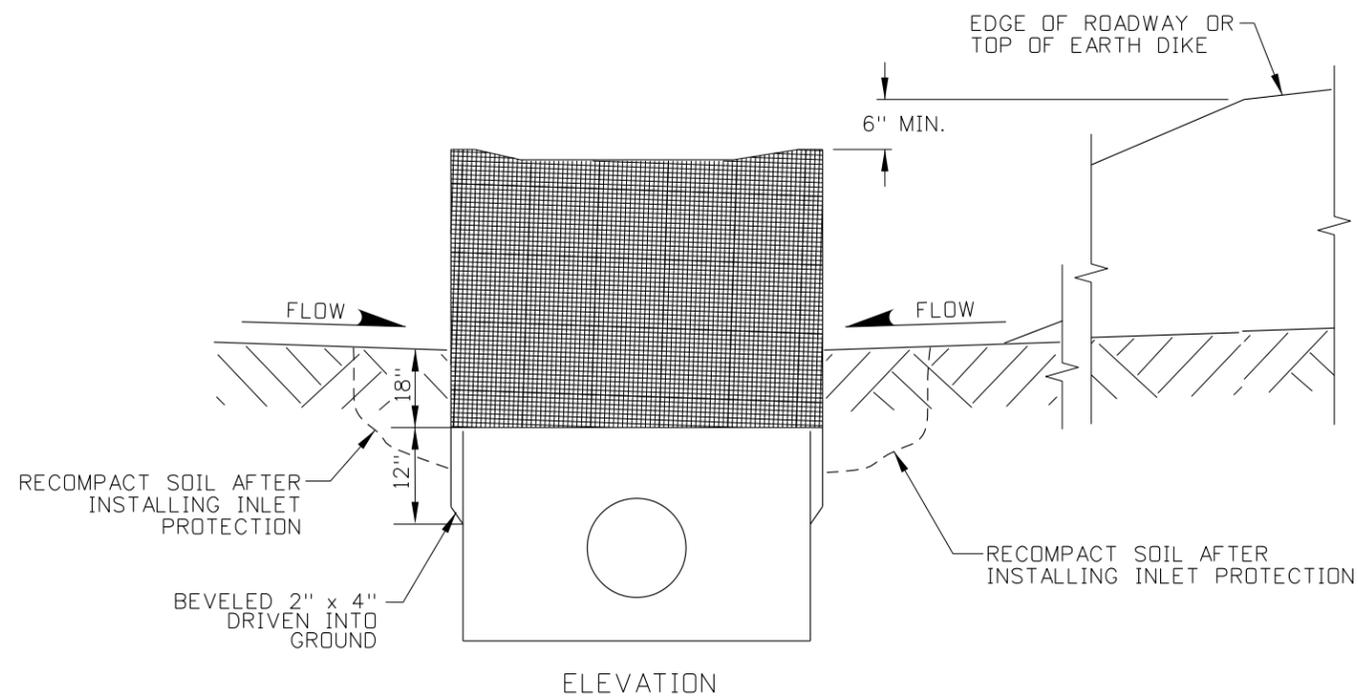
ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
EROSION AND SEDIMENT CONTROL
 EXAMPLE APPLICATIONS

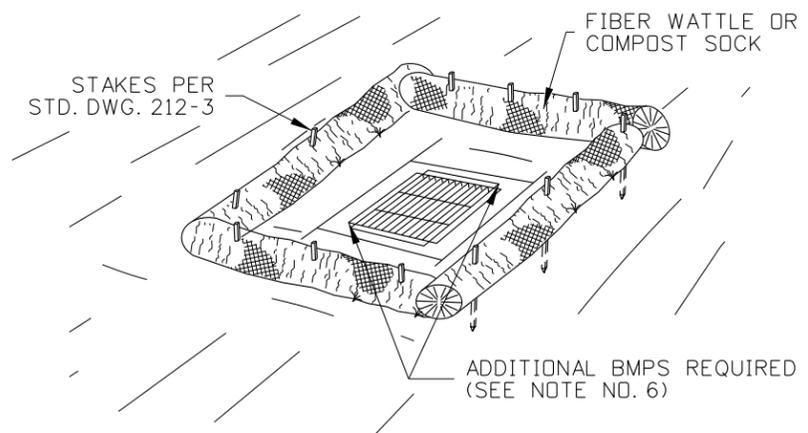
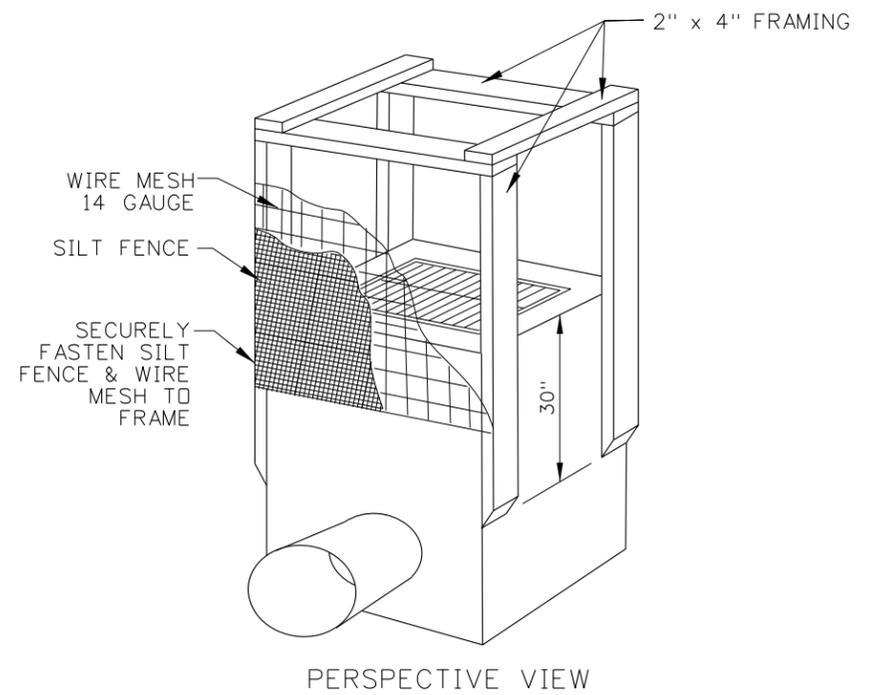
English
 STANDARD DRAWING NO.
 212-1
 SHEET 1 OF 1



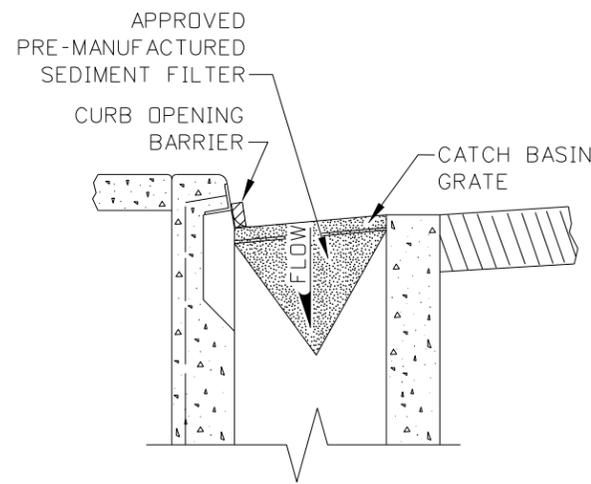
PROFESSIONAL ENGINEER
 LICENSED
 13683
 RYAN D. LANCASTER
 STATE OF IDAHO



FRAMED WIRE/FABRIC FILTER



FIBER WATTLE FILTER



PRE-MANUFACTURED SEDIMENT FILTER FOR INLET GRATE

NOTES

1. SEE THE GENERAL NOTES FOR EROSION CONTROL STANDARD DRAWINGS ON 212-1.
2. REMOVE TRASH, DEBRIS, DUFF, AND MATERIALS THAT MAY INTERFERE WITH THE INLET OR CATCH BASIN PROTECTION FUNCTION PRIOR TO PLACEMENT AND DAILY THEREAFTER OR AS NEEDED.
3. ADJUST TO ENSURE EFFECTIVENESS.
4. FRAMED WIRE/FABRIC FILTER AND FIBER WATTLE FILTERS ARE INTENDED TO BE USED ON STRUCTURES NOT PRESENTLY SURROUNDED BY PAVEMENT.
5. ENSURE WATER DISCHARGING FROM THE INLET MEETS APPLICABLE WATER QUALITY STANDARDS.
6. USE IN CONJUNCTION WITH OTHER INLET PROTECTION DEVICES, NOT AS THE SOLE BMP. SEE SECTION SC-6 IN THE BMP MANUAL.
7. DRAWING NOT TO SCALE.

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	10-10	KEH						
2	10-11	KEH						
3	01-13	RDL						
4	03-21	TWF						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 212-07_0421.dgn
 DRAWING DATE: JUNE, 1996

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
TEMPORARY EROSION AND SEDIMENT CONTROL INLET PROTECTION
 REQUIRES STD. DWG. 212-1

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

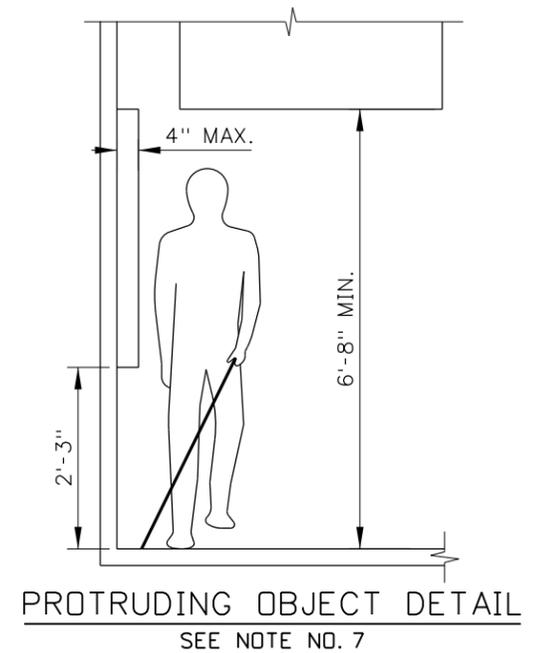
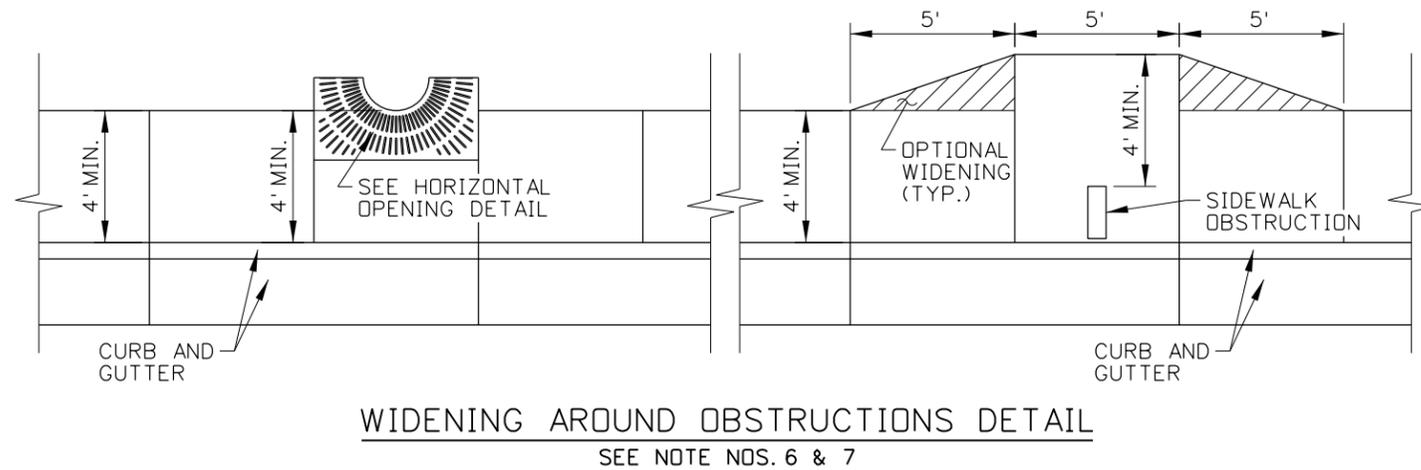
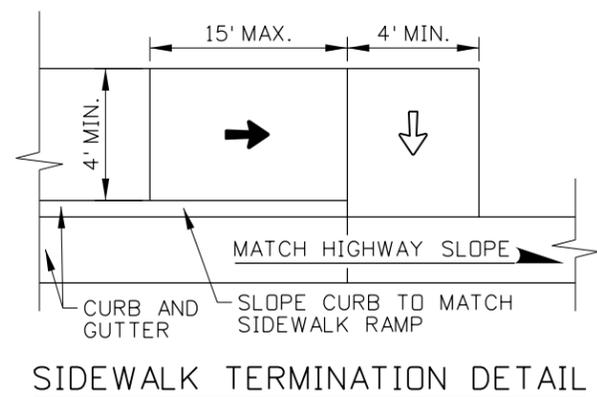
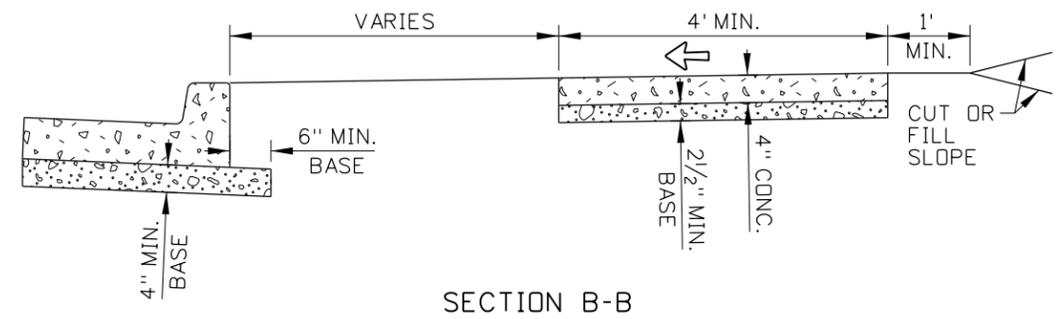
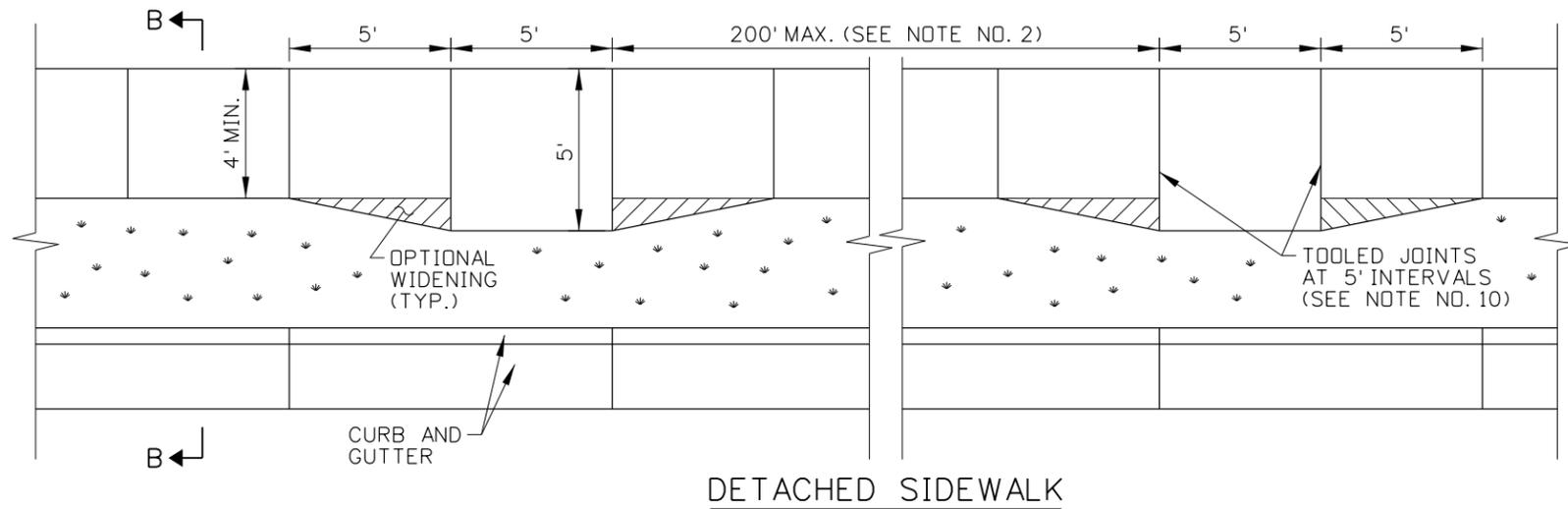
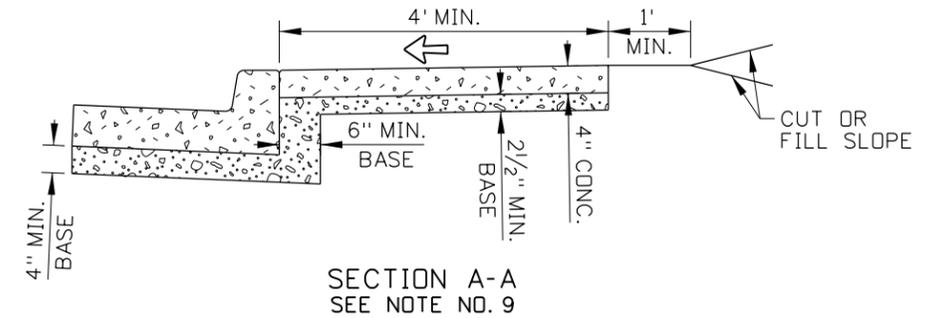
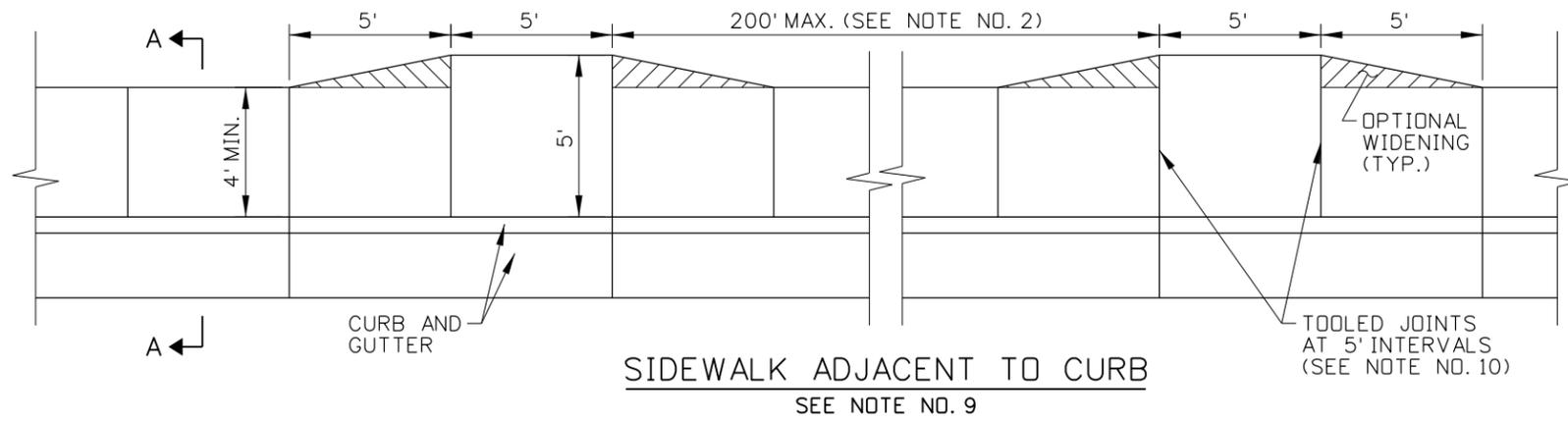
STANDARD DRAWING NO. **212-7**

SHEET 1 OF 1



SYMBOL LEGEND

- ↖ 1.0% TO 2.0% SLOPE
- ↙ 5.0% TO 8.3% RUNNING SLOPE, 2.0% OR FLATTER CROSS SLOPE



REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME: 614-1_0615.dgn
DRAWING DATE: MAY, 2015

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
SIDEWALKS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

STANDARD DRAWING NO. **614-1**

SHEET 1 OF 2

PROFESSIONAL ENGINEER
LICENSED
RYAN D. LANCASTER
STATE OF IDAHO
JUN 15, 2011

SYMBOL LEGEND

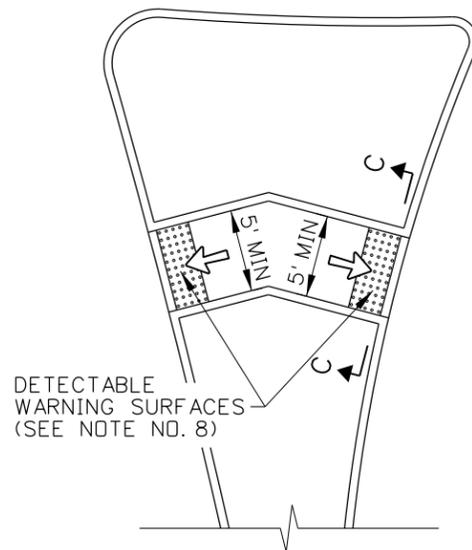
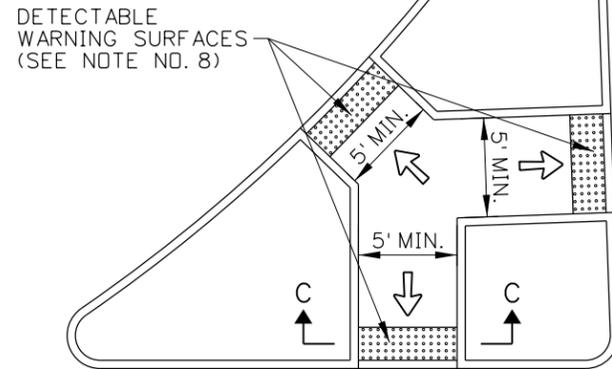
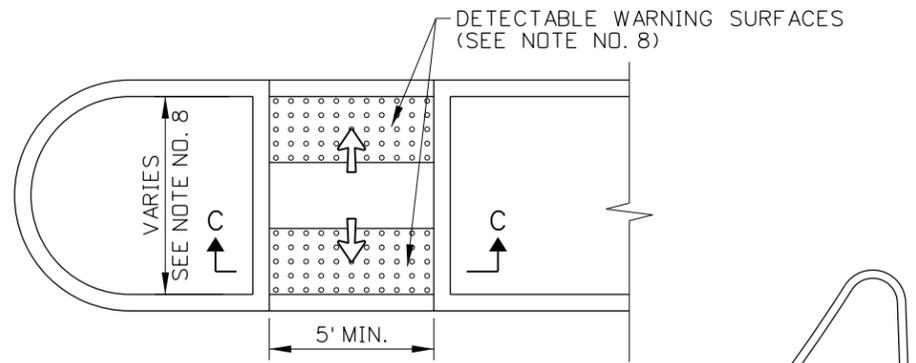
- ↔ 1.0% TO 2.0% SLOPE
- ↔ 5.0% TO 8.3% RUNNING SLOPE,
2.0% OR FLATTER CROSS SLOPE

NOTES

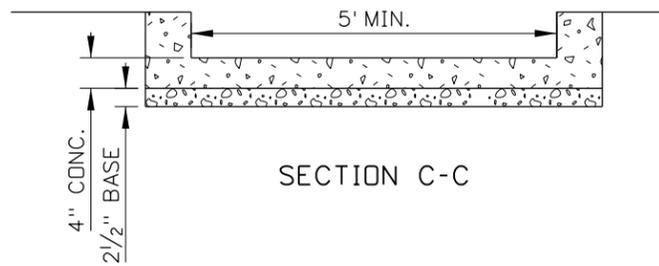
1. SIDEWALKS MAY CONSIST OF A PEDESTRIAN CIRCULATION PATH AND A PEDESTRIAN ACCESS ROUTE. THE PEDESTRIAN CIRCULATION PATH IS A PREPARED SURFACE PROVIDED FOR PEDESTRIAN TRAVEL IN THE PUBLIC RIGHT-OF-WAY. THE PEDESTRIAN ACCESS ROUTE IS A CONTINUOUS AND UNOBSTRUCTED PATH OF TRAVEL PROVIDED FOR PEDESTRIANS WITH DISABILITIES WITHIN OR COINCIDING WITH A PEDESTRIAN CIRCULATION PATH.
2. PROVIDE AT LEAST 4' OF CONTINUOUS CLEAR WIDTH OF PEDESTRIAN ACCESS ROUTE, EXCLUSIVE OF THE CURB WIDTH. WHERE SIDEWALKS ARE WIDER THAN 4', ONLY A PORTION OF THE SIDEWALK IS REQUIRED TO BE PART OF THE PEDESTRIAN ACCESS ROUTE.

PROVIDE A PASSING SPACE AT 200' OR SHORTER INTERVALS WHEN THE CLEAR WIDTH OF THE PEDESTRIAN ACCESS ROUTE IS LESS THAN 5'. ENSURE THAT THE DIMENSIONS OF THE PASSING SPACE ARE AT LEAST 5' BY 5'. INTERSECTING SIDEWALKS, DRIVEWAYS, AND ALLEYS MAY BE USED AS PASSING SPACES.
3. ENSURE THAT THE GRADE OF THE PEDESTRIAN ACCESS ROUTE DOES NOT EXCEED THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT HIGHWAY.
4. ENSURE THAT THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE WITHIN THE SIDEWALK DOES NOT EXCEED TWO PERCENT.
5. VERTICAL SURFACE DISCONTINUITIES MAY OCCASIONALLY OCCUR AT EXPANSION JOINTS, UTILITY COVERS, VAULT FRAMES, AND GRATINGS WITHIN THE SIDEWALK. ENSURE THAT VERTICAL SURFACE DISCONTINUITIES DO NOT EXCEED 1/2". BEVEL VERTICAL SURFACE DISCONTINUITIES BETWEEN 1/4" AND 1/2" WITH A 2:1 SLOPE ACROSS THE ENTIRE VERTICAL SURFACE DISCONTINUITY.
6. ENSURE THAT HORIZONTAL OPENINGS IN GRATINGS AND JOINTS DO NOT PERMIT PASSAGE OF A SPHERE MORE THAN 1/2" IN DIAMETER.
7. OBJECTS PROTRUDING INTO OR OVERHANGING A PEDESTRIAN CIRCULATION PATH MUST NOT REDUCE THE MINIMUM CLEAR WIDTH OF THE PEDESTRIAN ACCESS ROUTE. PROTRUDING OBJECTS INCLUDE STREET FURNITURE, STREET LIGHTS, UTILITY POLES, EQUIPMENT CABINETS, SIGN POSTS AND SIGNS, PARKING METERS, TRASH RECEPTACLES, PUBLIC TELEPHONES, MAILBOXES, NEWSPAPER VENDING MACHINES, BENCHES, TRANSIT SHELTERS, KIOSKS, BICYCLE RACKS, PLANTERS AND PLANTED TREES, AND STREET SCULPTURES.

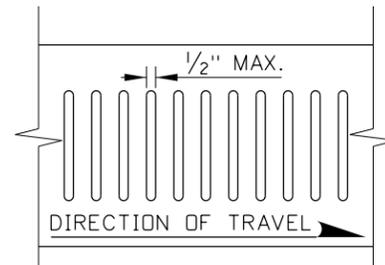
ENSURE THAT OBJECTS WITH LEADING EDGES BETWEEN 2'-3" AND 6'-8" ABOVE THE FINISH SURFACE DO NOT PROTRUDE MORE THAN 4" HORIZONTALLY INTO THE PEDESTRIAN CIRCULATION PATH.
8. PROVIDE DETECTABLE WARNING SURFACES ON PEDESTRIAN REFUGE ISLANDS WITH CURB RAMPS OR WHEN CUT-THROUGH AT STREET LEVEL AND REFUGE ISLAND WIDTHS ARE GREATER THAN SIX FEET IN THE DIRECTION OF PEDESTRIAN TRAVEL. DO NOT INSTALL DETECTABLE WARNING SURFACES AT PEDESTRIAN REFUGE ISLANDS THAT ARE CUT-THROUGH AT STREET LEVEL AND ARE LESS THAN SIX FEET IN WIDTH IN THE DIRECTION OF PEDESTRIAN TRAVEL. SEE STANDARD DRAWING 614-3 FOR DETECTABLE WARNING SURFACE DETAILS.
9. USE A BOND PREVENTATIVE BETWEEN THE SIDEWALK AND CURB WHEN CONSTRUCTED SEPARATELY AND PLACED ADJACENT TO EACH OTHER.
10. ALIGN CURB AND SIDEWALK JOINTS. CONSTRUCT JOINTS AT 5' INTERVALS THAT ARE APPROXIMATELY 1/8" WIDE AND 3/4" IN DEPTH. CONSTRUCT A LONGITUDINAL JOINT WHEN THE SIDEWALK IS 8' WIDE OR WIDER. INSTALL A PREFORMED EXPANSION JOINT FILLER EVERY 40'.
11. DRAWING NOT TO SCALE.



PEDESTRIAN REFUGE ISLAND DETAILS

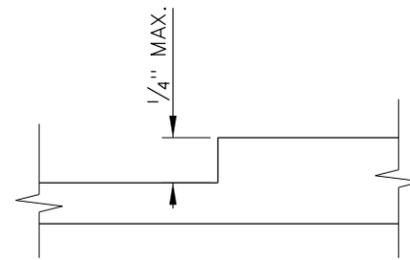


SECTION C-C

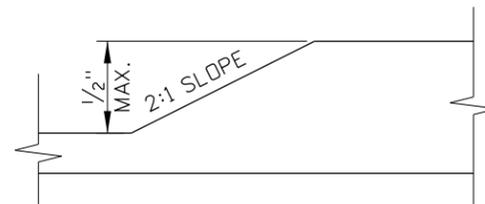


HORIZONTAL OPENING DETAIL

SEE NOTE NO. 6



VERTICAL DISCONTINUITY OF 1/4" OR LESS



VERTICAL DISCONTINUITY BETWEEN 1/4" AND 1/2"

VERTICAL SURFACE DISCONTINUITY DETAIL

SEE NOTE NO. 5

REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME: 614-1_0615.dgn
DRAWING DATE: MAY, 2015

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

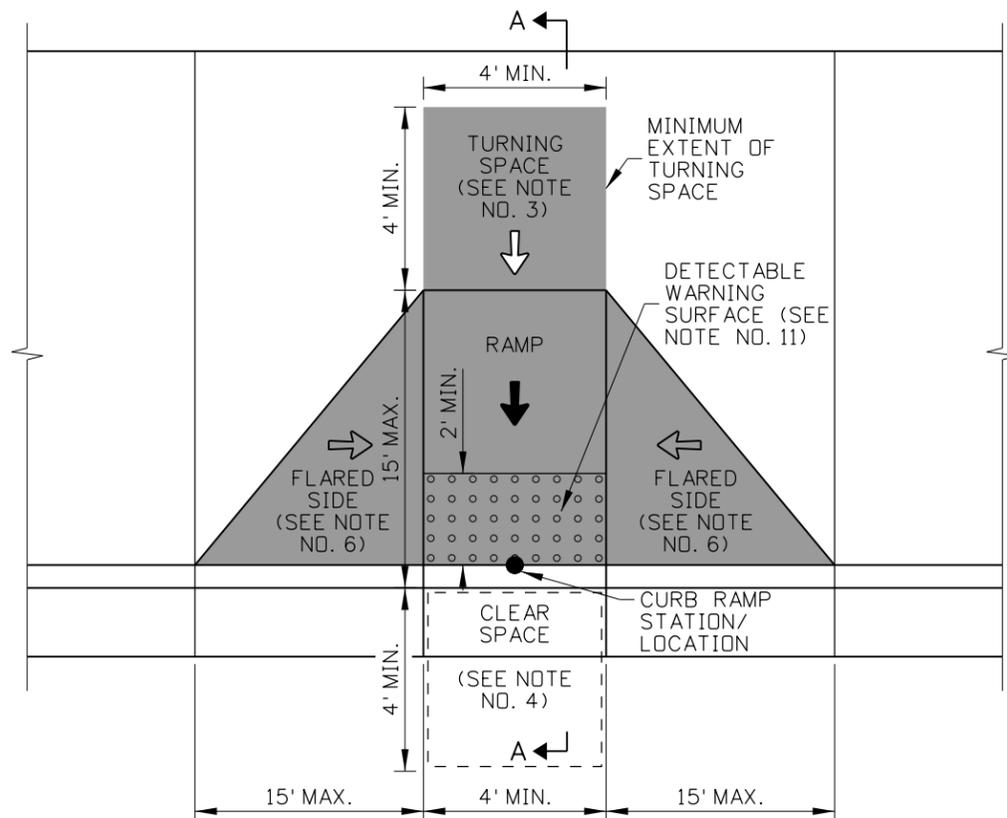
STANDARD DRAWING
SIDEWALKS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

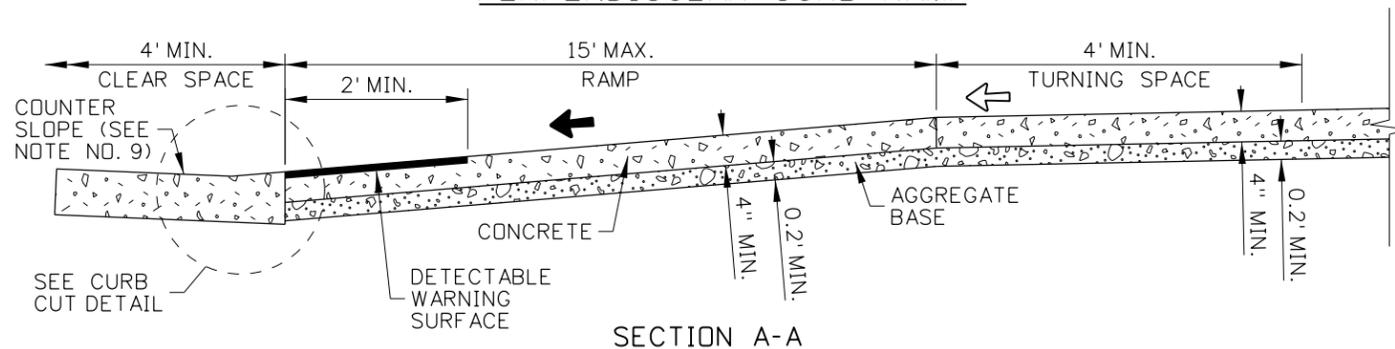
English

STANDARD DRAWING NO. **614-1**

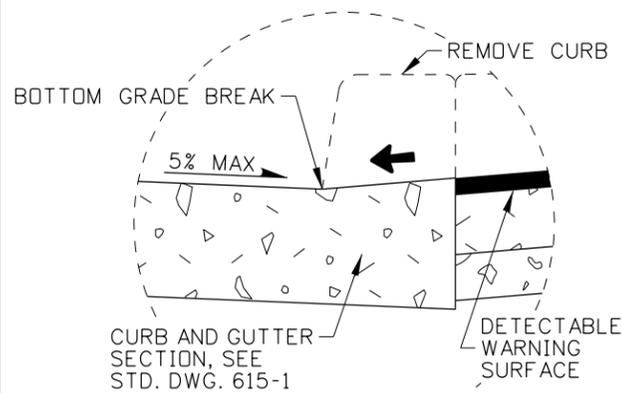
SHEET 2 OF 2



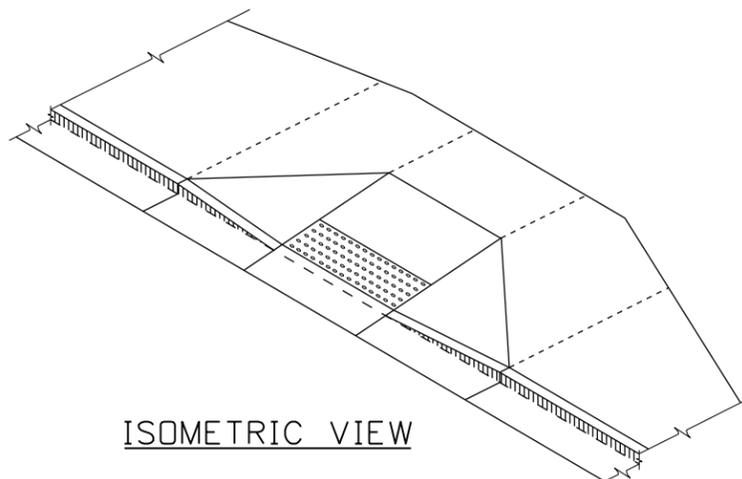
PERPENDICULAR CURB RAMP



SECTION A-A



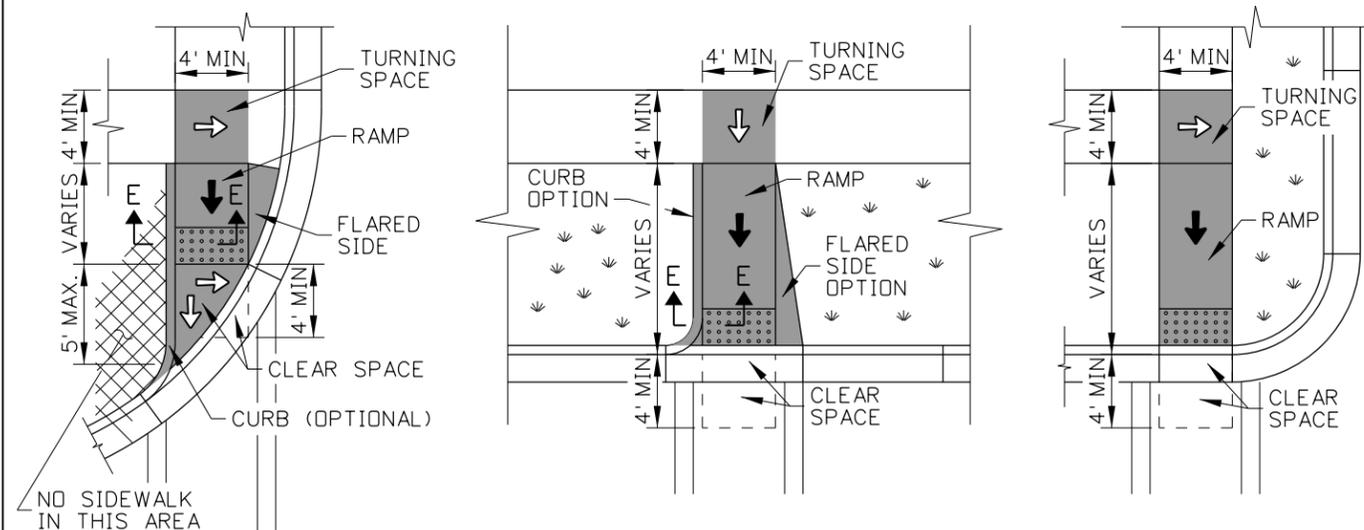
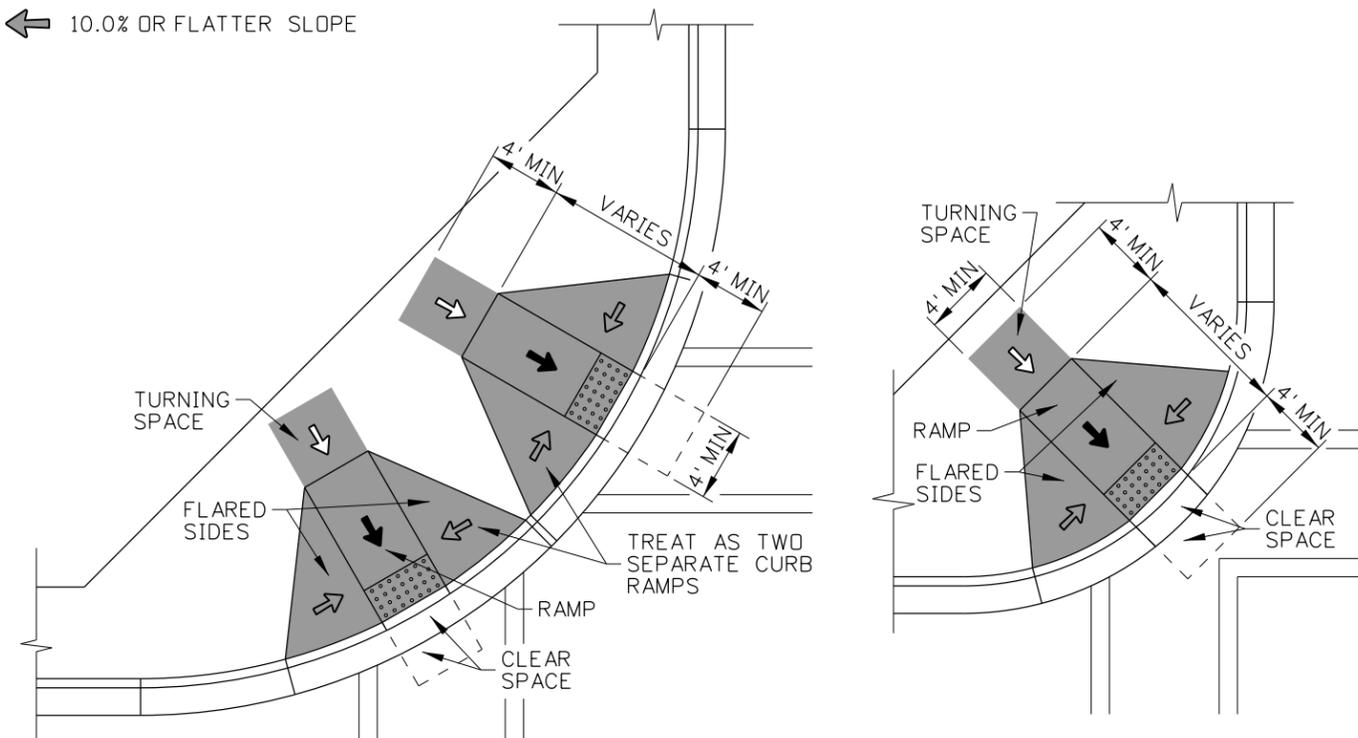
CURB CUT DETAIL



ISOMETRIC VIEW

SYMBOL LEGEND

- ↔ 1.0% TO 2.0% SLOPE
- ↖ 5.0% TO 8.3% RUNNING SLOPE, 2.0% OR FLATTER CROSS SLOPE
- ↗ 10.0% OR FLATTER SLOPE



EXAMPLE APPLICATIONS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	09-93	MSM	6	07-03	MSM	11	07-10	JAW
2	12-95	MSM	7	12-04	MSM	12	09-11	TEM
3	06-98	MSM	8	06-05	MSM	13	05-15	RDL
4	08-01	MSM	9	05-06	MSM			
5	10-02	MSM	10	05-07	MSM			

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 614-3_0615.dgn
 DRAWING DATE: JUNE, 1990

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING

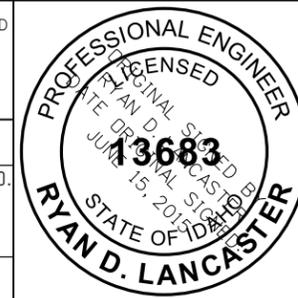
CURB RAMPS

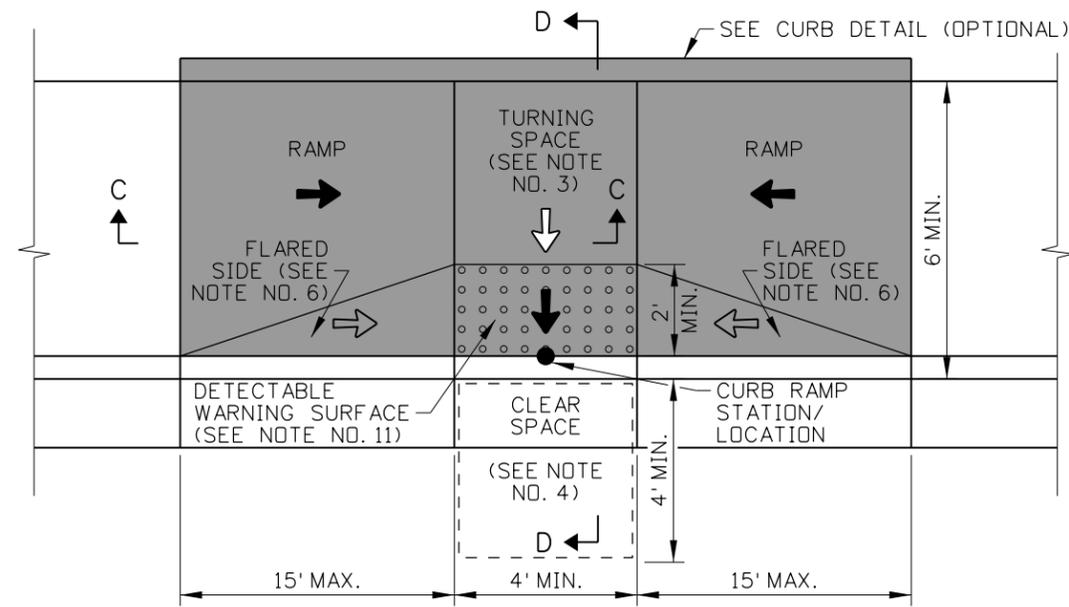
English

STANDARD DRAWING NO.

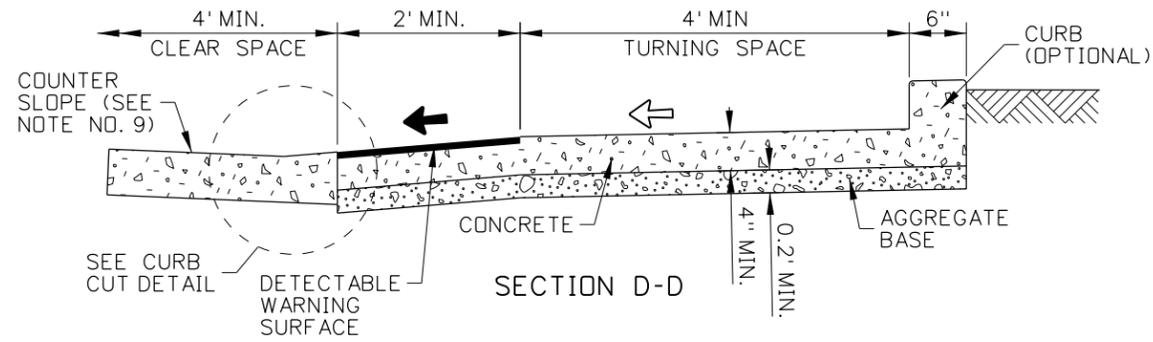
614-3

SHEET 1 OF 4

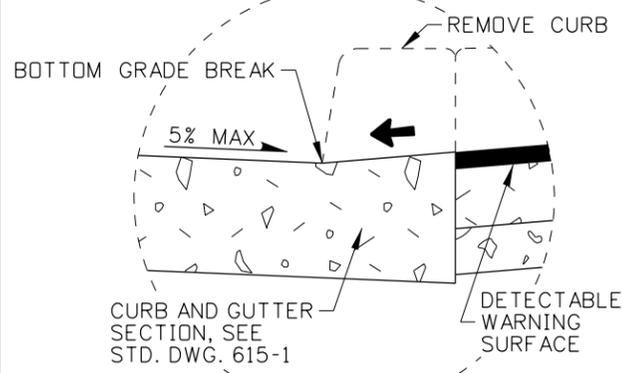




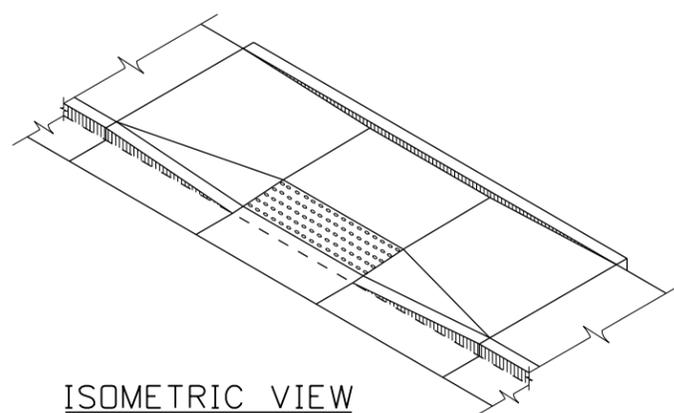
COMBINATION CURB RAMP



SECTION D-D



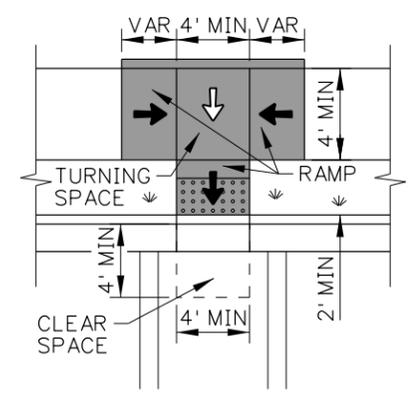
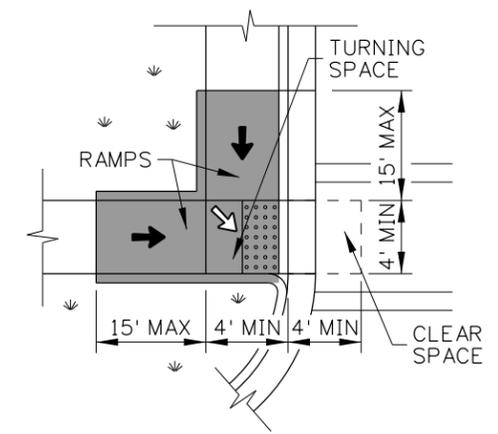
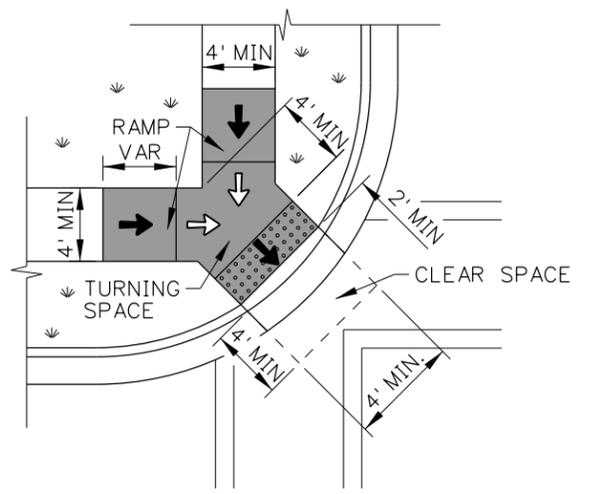
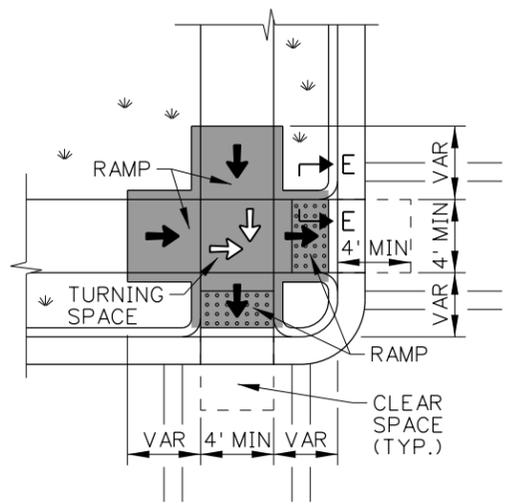
CURB CUT DETAIL



ISOMETRIC VIEW

SYMBOL LEGEND

- ↔ 1.0% TO 2.0% SLOPE
- ↖ 5.0% TO 8.3% RUNNING SLOPE, 2.0% OR FLATTER CROSS SLOPE
- ↗ 10.0% OR FLATTER SLOPE



EXAMPLE APPLICATIONS

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	09-93	MSM	6	07-03	MSM	11	07-10	JAW
2	12-95	MSM	7	12-04	MSM	12	09-11	TEM
3	06-98	MSM	8	06-05	MSM	13	05-15	RDL
4	08-01	MSM	9	05-06	MSM			
5	10-02	MSM	10	05-07	MSM			

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 614-3_0615.dgn

DRAWING DATE: JUNE, 1990

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING

CURB RAMPS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

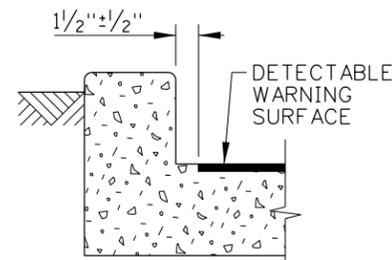
STANDARD DRAWING NO. **614-3**

SHEET 3 OF 4

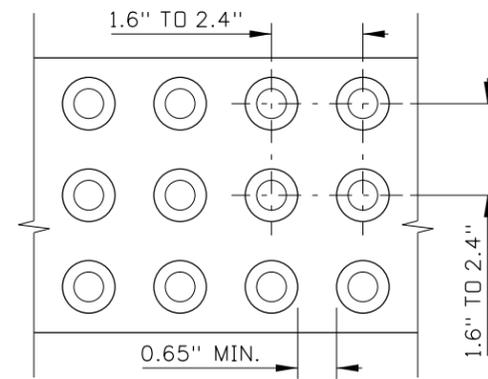
PROFESSIONAL ENGINEER
LICENSED
RYAN D. LANCASTER
STATE OF IDAHO
JUN 15, 2011
13683

SYMBOL LEGEND

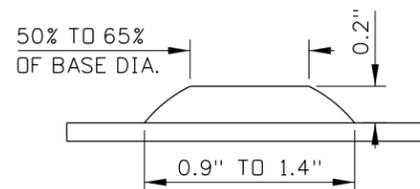
- ← 1.0% TO 2.0% SLOPE
- ← 5.0% TO 8.3% RUNNING SLOPE, 2.0% OR FLATTER CROSS SLOPE
- ← 10.0% OR FLATTER SLOPE



SECTION E-E

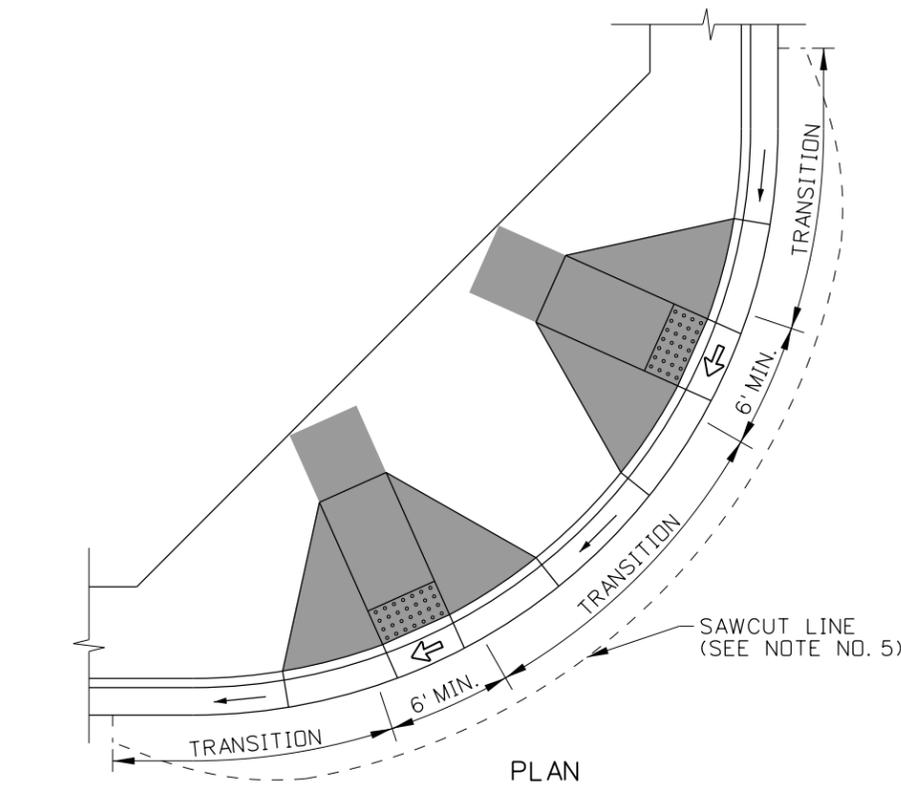


DOME SPACING

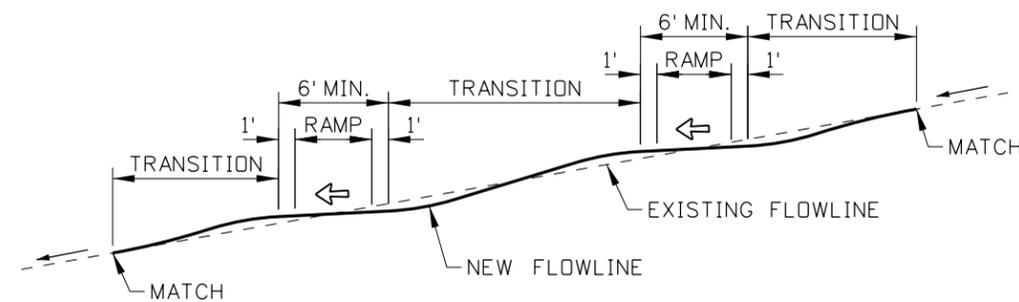


DOME SIZE

DETECTABLE WARNING SURFACE DETAILS
SEE NOTE NO. 11



PLAN



PROFILE

FLOWLINE PROFILE DETAIL
SEE NOTE NO. 5

NOTES

1. EXTENTS OF CURB RAMP PAY ITEMS ARE SHOWN IN GRAY SHADING.
2. CURB RAMP CAN BE PERPENDICULAR, PARALLEL, OR A COMBINATION OF PARALLEL AND PERPENDICULAR RAMP. EXAMPLE APPLICATIONS OF EACH ARE SHOWN ON SHEETS 1, 2, AND 3.
PERPENDICULAR CURB RAMP: PERPENDICULAR CURB RAMP HAS A RAMP THAT CUTS THROUGH THE CURB AT RIGHT ANGLES OR MEETS THE GUTTER GRADE BREAK AT RIGHT ANGLES WHEN THE CURB IS CURVED.
PARALLEL CURB RAMP: PARALLEL CURB RAMP HAS A RAMP OR RAMPS IN-LINE WITH THE DIRECTION OF SIDEWALK TRAVEL AND LOWER THE SIDEWALK TO A LEVEL TURNING SPACE WHERE A TURN IS MADE TO ENTER THE PEDESTRIAN STREET CROSSING.
COMBINATION CURB RAMP: COMBINATION CURB RAMP HAS FEATURES FROM PERPENDICULAR AND PARALLEL CURB RAMP.
3. PROVIDE A TURNING SPACE WITH A 2.0% OR FLATTER SLOPE IN EACH DIRECTION. TURNING SPACES MAY OVERLAP WITH OTHER TURNING SPACES AND CLEAR SPACES.
PERPENDICULAR CURB RAMP: PROVIDE A 4' BY 5' MINIMUM TURNING SPACE WHEN THE TURNING SPACE IS CONSTRAINED AT THE BACK-OF-SIDEWALK.
PARALLEL CURB RAMP: PROVIDE A 4' BY 5' TURNING SPACE WHEN THE TURNING SPACE IS CONSTRAINED ON TWO OR MORE SIDES. ENSURE THAT THE 5' DIMENSION IS PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
4. PROVIDE A CLEAR SPACE BEYOND THE BOTTOM OF THE GRADE BREAK THAT IS WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAFFIC LANE.
5. CROSS SLOPE IS THE SLOPE PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL. ENSURE THAT THE CROSS SLOPE OF THE RAMP AND TURNING SPACE DOES NOT EXCEED TWO PERCENT. AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL AND AT MIDBLOCK PEDESTRIAN STREET CROSSINGS, THE CROSS SLOPE MAY MATCH THE STREET OR HIGHWAY GRADE. FLATTEN THE GUTTER FLOWLINE THROUGH CURB RAMP TO TWO PERCENT OR FLATTER WHEN NEEDED. WHEN THE PAVEMENT IS SAWCUT TO FLATTEN THE FLOWLINE, VARY THE WIDTH OF THE SAWCUT SO THAT THE PAVEMENT PATCH SMOOTHLY MATCHES THE EXISTING PAVEMENT.
6. PROVIDE FLARED SIDES ON PERPENDICULAR CURB RAMP, OR COMBINATION CURB RAMP WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. THE FLARED SIDES ARE PART OF THE PEDESTRIAN CIRCULATION PATH, BUT ARE NOT PART OF THE PEDESTRIAN ACCESS ROUTE. THE SLOPE OF THE FLARED SIDES IS MEASURED PARALLEL TO THE CURB LINE. FLARED SIDES ARE NOT NEEDED OR MAY BE STEEPER WHEN THE PEDESTRIAN CIRCULATION PATH DOES NOT CROSS THE CURB RAMP.
7. THE PEDESTRIAN CIRCULATION PATH IS A PREPARED SURFACE PROVIDED FOR PEDESTRIAN TRAVEL IN THE PUBLIC RIGHT-OF-WAY. THE PEDESTRIAN ACCESS ROUTE IS A CONTINUOUS AND UNOBSTRUCTED PATH OF TRAVEL PROVIDED FOR PEDESTRIANS WITH DISABILITIES WITHIN OR COINCIDING WITH A PEDESTRIAN CIRCULATION PATH.
8. ENSURE THAT GRADE BREAKS ARE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN AND ARE FLUSH. DO NOT CREATE GRADE BREAKS ON THE SURFACE OF RAMP RUNS AND TURNING SPACES.
9. ENSURE THAT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS DOES NOT EXCEED FIVE PERCENT.
10. WHERE PRACTICAL, PLACE UTILITY COVERS, VAULT FRAMES, AND GRATINGS OUTSIDE RAMP RUNS, TURNING SPACES, OR GUTTER AREAS. LOCATE CATCH BASINS AND INLETS OUTSIDE OF RAMP RUNS.
11. DETECTABLE WARNING SURFACES CONSIST OF TRUNCATED DOMES ALIGNED IN A SQUARE OR RADIAL GRID PATTERN. PROVIDE DETECTABLE WARNING SURFACES THAT CONTRAST VISUALLY WITH ADJACENT GUTTER, HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT. ENSURE THAT THE DETECTABLE WARNING SURFACE EXTENDS THE FULL WIDTH OF THE RAMP RUN (EXCLUDING FLARED SIDES) OR TURNING SPACE.
PERPENDICULAR AND COMBINATION CURB RAMP: WHERE THE ENDS OF THE BOTTOM GRADE BREAK ARE IN FRONT OF THE BACK OF CURB, PLACE THE DETECTABLE WARNING SURFACE AT THE BACK OF CURB.
WHERE THE ENDS OF THE BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB, PLACE THE DETECTABLE WARNING SURFACE ON THE RAMP RUN WITHIN ONE DOME SPACING OF THE BOTTOM GRADE BREAK AND WITHIN 5' OF THE BACK OF CURB.
PARALLEL CURB RAMP: PLACE DETECTABLE WARNING SURFACE ON THE TURNING SPACE AT THE BACK OF CURB.
12. USE A BOND PREVENTATIVE BETWEEN THE CURB RAMP OR SIDEWALK AND CURB WHEN CONSTRUCTED SEPARATELY AND PLACED ADJACENT TO EACH OTHER.
13. ALIGN ALTERNATING CURB AND SIDEWALK JOINTS. CONSTRUCT JOINTS APPROXIMATELY 1/8" WIDE AND 3/4" IN DEPTH.
14. DRAWING NOT TO SCALE.

REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE
1	09-93	MSM	6	07-03	MSM	11	07-10
2	12-95	MSM	7	12-04	MSM	12	09-11
3	06-98	MSM	8	06-05	MSM	13	05-15
4	08-01	MSM	9	05-06	MSM		
5	10-02	MSM	10	05-07	MSM		

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME: 614-3_0615.dgn
DRAWING DATE: JUNE, 1990

IDAHO TRANSPORTATION DEPARTMENT

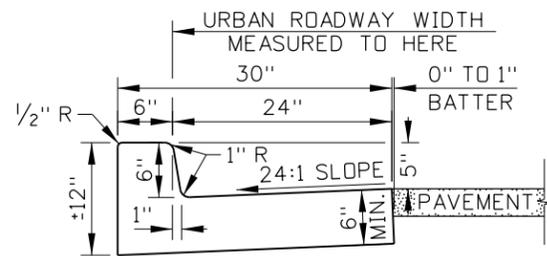
BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

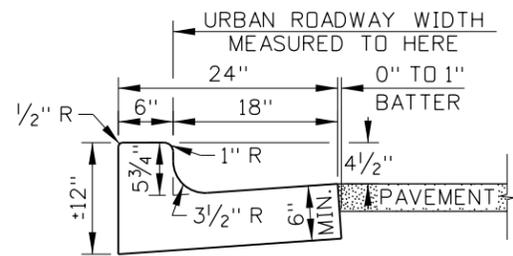
STANDARD DRAWING
CURB RAMPS

English
STANDARD DRAWING NO.
614-3
SHEET 4 OF 4

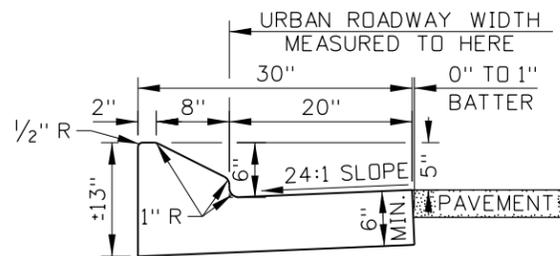
ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



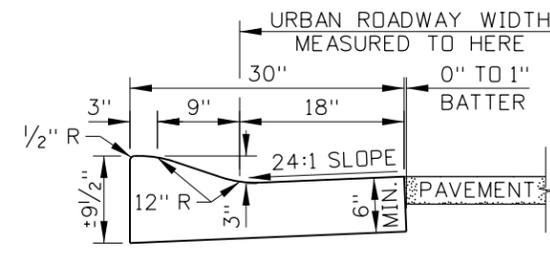
CURB AND GUTTER TYPE 1



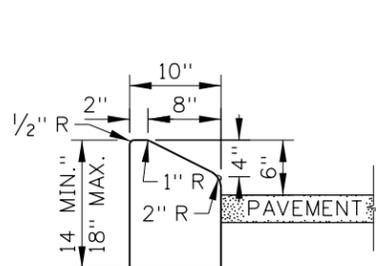
CURB AND GUTTER TYPE 2
(SEE NOTE NO. 4)



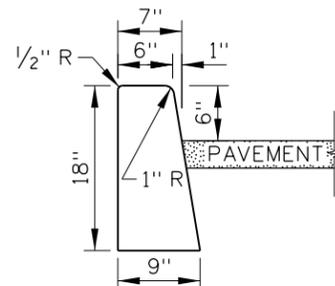
CURB AND GUTTER TYPE 3



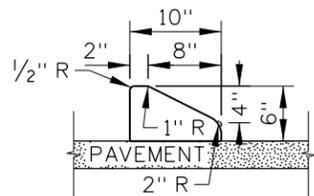
CURB AND GUTTER TYPE 4



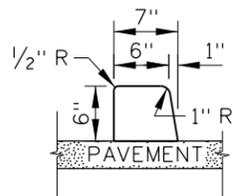
CURB TYPE 1



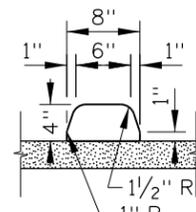
CURB TYPE 2



CURB TYPE 3
(SEE NOTE NO. 3)



CURB TYPE 4
(SEE NOTE NO. 3)



CURB TYPE 5
(SEE NOTE NO. 3)

NOTES

1. TRANSITION BETWEEN DIFFERENT TYPES OVER 10 FEET.
2. PROVIDE 4 INCHES OF AGGREGATE BASE UNDER CURB AND GUTTER, CURB, OR GUTTER UNLESS THE CURB IS PLACED ON PAVEMENT.
3. PORTLAND CEMENT CONCRETE CURB OR TRAFFIC SEPARATOR ON ASPHALT CONCRETE PAVEMENT:

PROVIDE A KEY IN THE PAVEMENT AT THE CENTERLINE OF THE CURB OR TRAFFIC SEPARATOR. SEE THE KEY DETAIL.

CONCRETE CURBS OR TRAFFIC SEPARATORS MAY BE PINNED TO THE PAVEMENT STRUCTURE IN LIEU OF THE KEY. DRILL THE PAVEMENT AND PLACE PINS BEFORE THE CURB OR TRAFFIC SEPARATOR IS CONSTRUCTED. SEE THE PIN DETAIL.

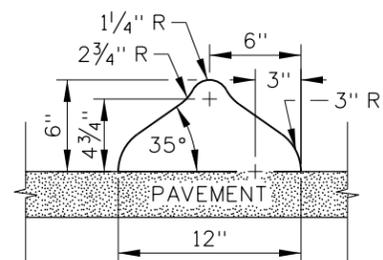
PORTLAND CEMENT CONCRETE CURB OR TRAFFIC SEPARATOR ON PORTLAND CEMENT CONCRETE PAVEMENT:

USE AN EPOXY BONDING AGENT. NO KEY IS NEEDED.

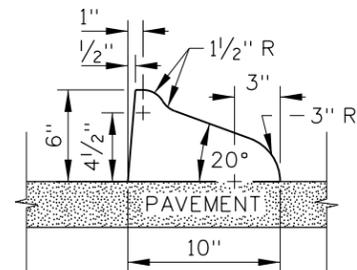
ASPHALT CONCRETE CURB OR TRAFFIC SEPARATOR ON ASPHALT CONCRETE PAVEMENT:

NO KEY IS NEEDED. ENSURE THAT THE CURB IS BONDED TO THE PAVEMENT.

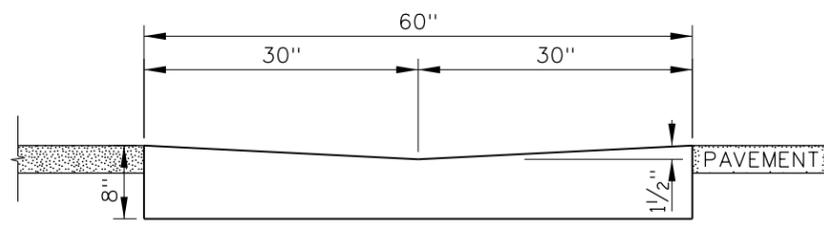
4. ENSURE THAT THE GUTTER SLOPE DOES NOT EXCEED 5 PERCENT AT CURB RAMPS.
5. TAPER THE LAST 6 FEET OF CURB AND GUTTER TYPES 1, 2, 3, AND 4 AND CURB TYPES 1 AND 2 DOWN TO A 1 INCH HEIGHT. TAPER CURB TYPES 3, 4, AND 5 AND TRAFFIC SEPARATORS TYPES 1 AND 2 DOWN TO A 1 INCH HEIGHT AT A 1:1 SLOPE.
6. DRAWING NOT TO SCALE.



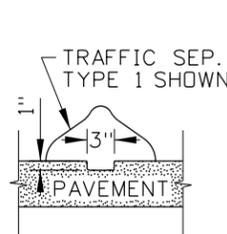
TRAFFIC SEPARATOR TYPE 1
(SEE NOTE NO. 3)



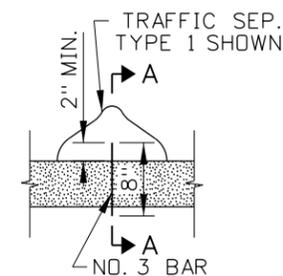
TRAFFIC SEPARATOR TYPE 2
(SEE NOTE NO. 3)



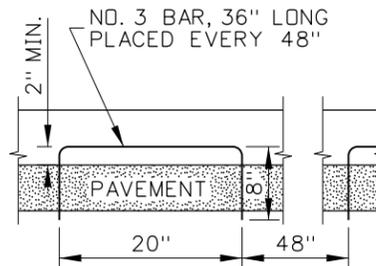
GUTTER TYPE 1



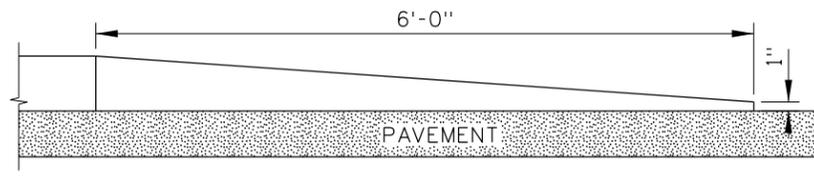
KEY DETAIL
(SEE NOTE NO. 3)



PIN DETAIL
(SEE NOTE NO. 3)

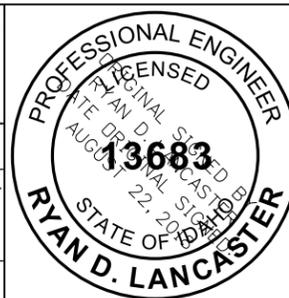


SECTION A-A



CURB TERMINUS DETAIL
(SEE NOTE NO. 5)

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	02-76		6	12-04	MSM	11	07-18	RDL
2	12-90	GB	7	06-05	MSM			
3	09-93	MSM	8	07-10	JAW			
4	12-94	MSM	9	11-14	RDL			
5	12-01	MSM	10	06-17	RDL			

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
CADD FILE NAME: 615-1_0818.dgn
DRAWING DATE: APRIL, 1961

IDAHO TRANSPORTATION DEPARTMENT
BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

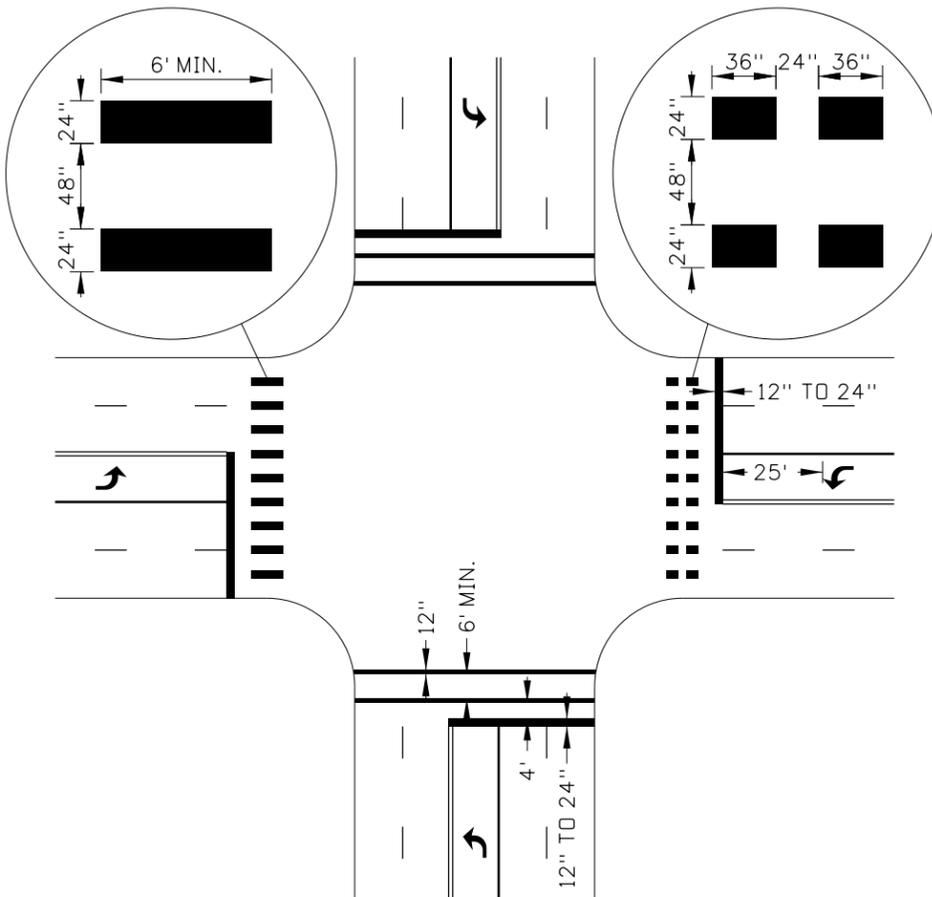
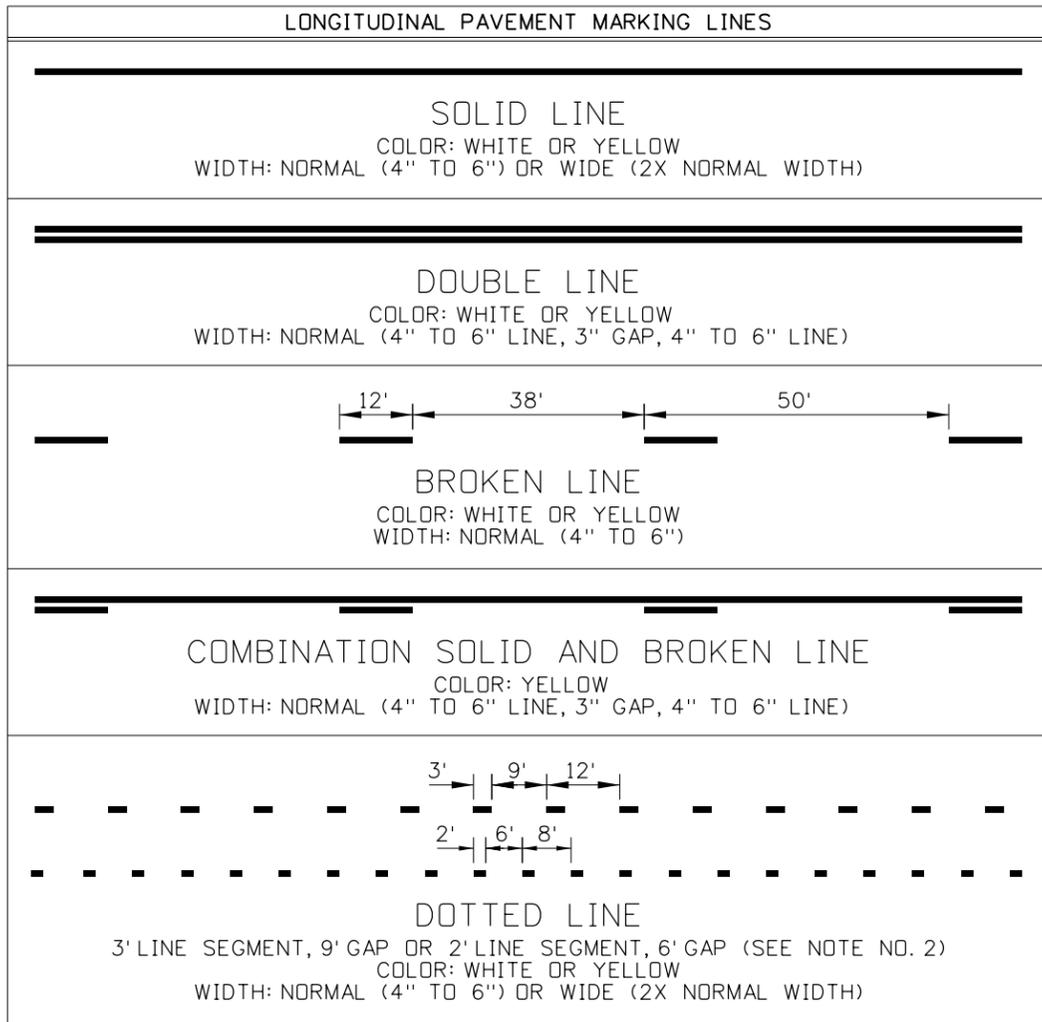
STANDARD DRAWING
CURB AND GUTTER

English
STANDARD DRAWING NO. 615-1
SHEET 1 OF 1

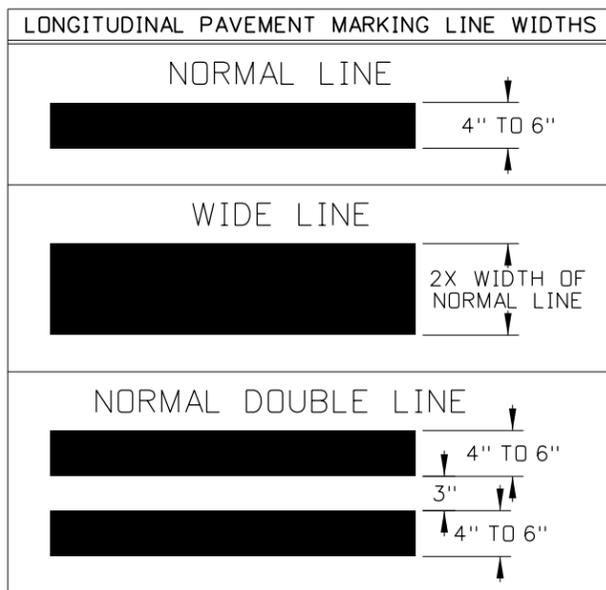
NOTES

1. USE WHITE AND YELLOW PAVEMENT MARKINGS AS FOLLOWS:
 - WHITE:
 - A. THE SEPARATION OF TRAFFIC TRAVELING IN THE SAME DIRECTION.
 - B. THE RIGHT-HAND EDGE OF THE HIGHWAY.
 - YELLOW:
 - A. THE SEPARATION OF TRAFFIC TRAVELING IN OPPOSITE DIRECTIONS.
 - B. THE LEFT-HAND EDGE DIVIDED HIGHWAYS, ONE-WAY STREETS, OR RAMP.
 - C. TWO-WAY LEFT-TURN LANES.
2. USE LONGITUDINAL PAVEMENT MARKINGS AS FOLLOWS:
 - A. USE SOLID LINES TO INDICATE THE LEFT OR RIGHT EDGE OF TRAVEL WAY OR TO DISCOURAGE LANE CHANGING.
 - B. USE DOUBLE LINES TO PROHIBIT PASSING OR LANE CHANGING.
 - C. USE BROKEN LINES TO INDICATE PASSING OR LANE CHANGING ARE PERMITTED. USE THE 12' LINE SEGMENT, 38' GAP PATTERN FOR ALL SPEEDS.
 - D. USE COMBINATION SOLID AND BROKEN LINES TO PROHIBIT PASSING IN ONE DIRECTION WHILE PERMITTING PASSING IN THE OPPOSITE DIRECTION OR TO INDICATE A TWO-WAY LEFT-TURN LANE.
 - E. USE DOTTED LINES AS FOLLOWS:
 - 3' LINE SEGMENT, 9' GAP:
 - I. TO SEPARATE A THROUGH LANE AND A LANE THAT BECOMES A MANDATORY EXIT OR TURN LANE (DROPPED LANE).
 - II. TO SEPARATE THROUGH LANES AND TURN LANES OR RAMP.
 - III. TO SEPARATE A THROUGH LANE AND AN AUXILIARY LANE 2 MILES OR LESS IN LENGTH BETWEEN FREEWAY ENTRANCE RAMP AND EXIT RAMP OR 1 MILE OR LESS IN LENGTH BETWEEN INTERSECTIONS.
 - 2' LINE SEGMENT, 6' GAP:
 - I. AS A LANE LINE EXTENSION THROUGH AN INTERSECTION.
3. USE 12' VEHICULAR TRAVEL LANES UNLESS OTHERWISE INDICATED. MEASURE LANE WIDTHS FROM THE CENTER OF LINE TO THE CENTER OF LINE.
4. THE PAVEMENT MARKING APPLICATION EXAMPLES PRESENTED SHOW COMMON APPLICATION. MODIFY AS NEEDED TO ACCOMMODATE OTHER SITUATIONS.
5. METHODS FOR DETERMINING TURN-LANE LENGTH ARE DESCRIBED IN THE ITD TRAFFIC MANUAL.
6. USE 15W FOR POSTED SPEED LIMITS OF 45 MPH OR GREATER. USE 8W FOR POSTED SPEED LIMITS OF 40 MPH OR LESS. W IS THE OFFSET WIDTH IN FEET.
7. USE DISTANCE L WHEN PRACTICAL. USE THE FOLLOWING EQUATION TO DETERMINE L:

$$L = WS$$
 WHERE:
 - W = OFFSET WIDTH IN FEET
 - S = POSTED SPEED LIMIT
8. USE LANE-USE ARROWS AND WORD PAVEMENT MARKINGS AS SHOWN. SOME MARKINGS ARE OPTIONAL.
 - A. USE TWO OR MORE LANE-USE ARROWS UNLESS THE TURN-LANE LENGTH IS LESS THAN 75 FEET. IF SHORTER THAN 75 FEET, THE DOWNSTREAM ARROW CAN BE OMITTED.
 - B. USE TWO-WAY LEFT-TURN ARROW MARKINGS NEAR THE BEGINNING OF A TWO-WAY LEFT-TURN LANE AND EVERY 1/2 MILE THEREAFTER.
9. BREAK EDGE AND LANE LINES AT INTERSECTIONS WITH MINOR ROADS. CONTINUE EDGE AND LANE LINES THROUGH DRIVEWAY APPROACHES.
10. ON TWO-LANE HIGHWAYS, PAINT THE CENTERLINE IN ONE DIRECTION IN ASCENDING STATION/MILEPOST DIRECTION AS SHOWN.
11. DRAWINGS NOT TO SCALE.



EXAMPLE STOP LINE AND CROSSWALK DETAIL



REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	03-20	RDL						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 630-1_0420.dgn

DRAWING DATE: DECEMBER, 2016

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING

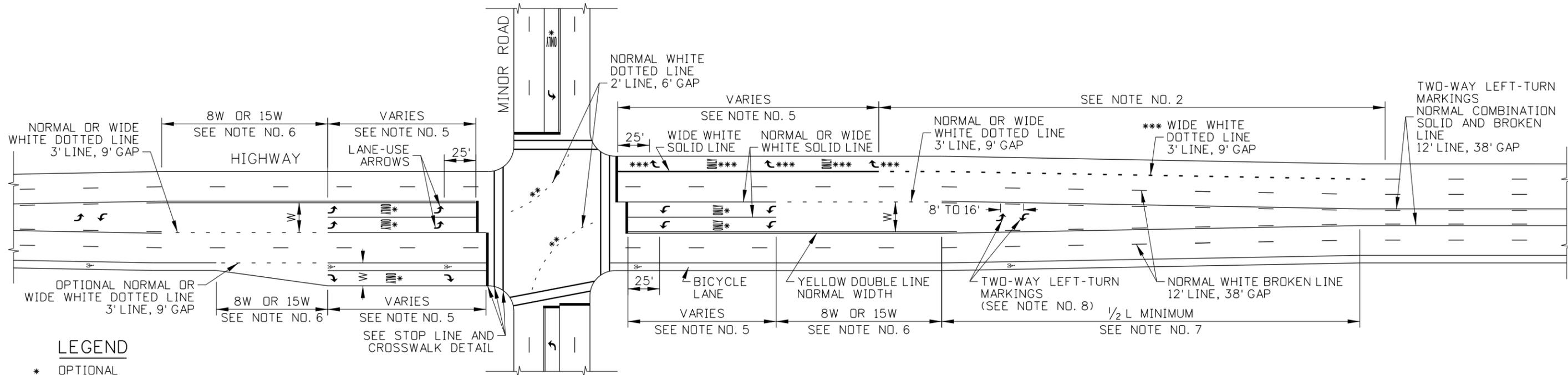
PAVEMENT MARKINGS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

STANDARD DRAWING NO. 630-1

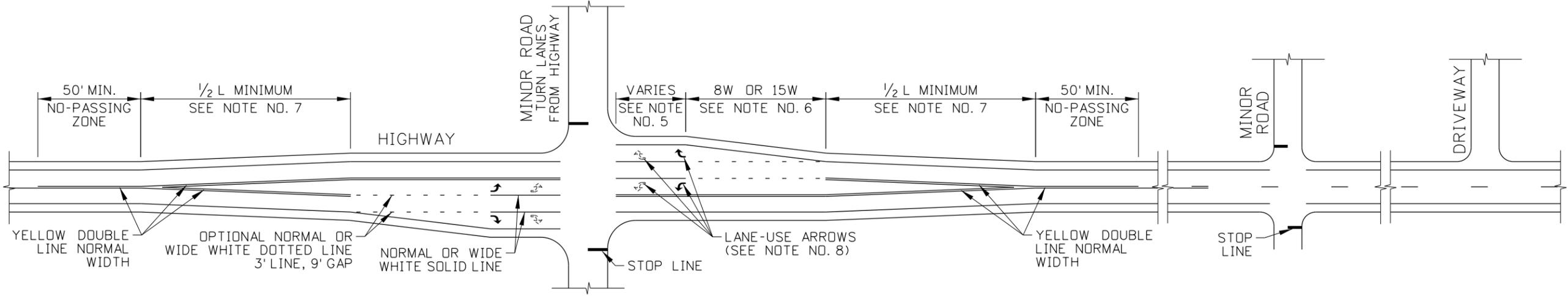
SHEET 1 OF 4



EXAMPLE URBAN HIGHWAY PAVEMENT MARKINGS

LEGEND

- * OPTIONAL
- ** DOTTED LANE LINE EXTENSION (2' SEGMENT, 6' GAP)
- *** REQUIRED WHERE THROUGH LANE BECOMES MANDATORY TURN LANE



EXAMPLE RURAL HIGHWAY PAVEMENT MARKINGS

SEE NOTE NO. 9

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	03-20	RDL						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 630-1_0420.dgn
 DRAWING DATE: DECEMBER, 2016

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

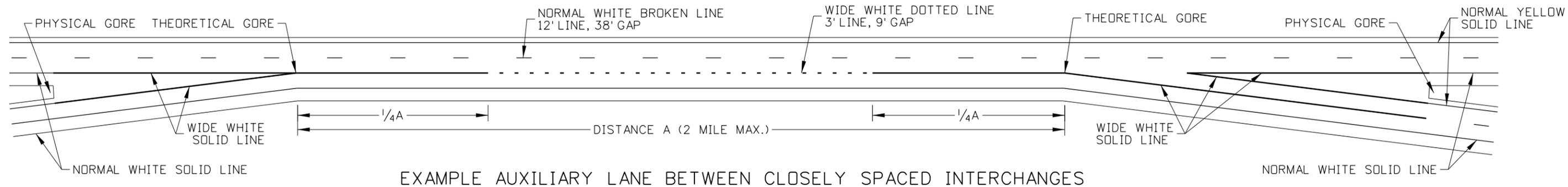
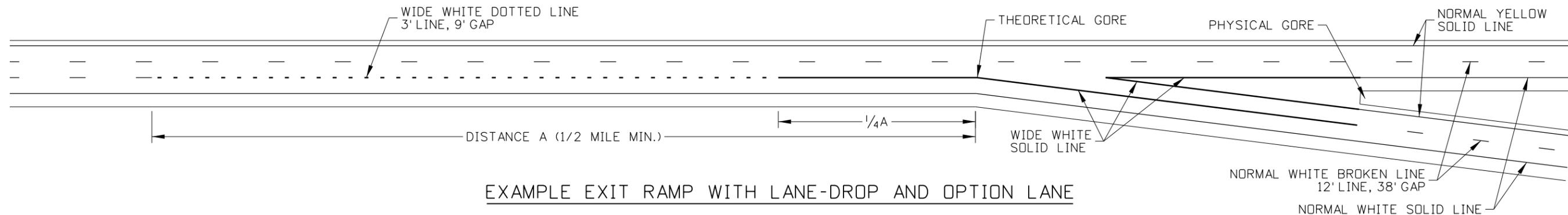
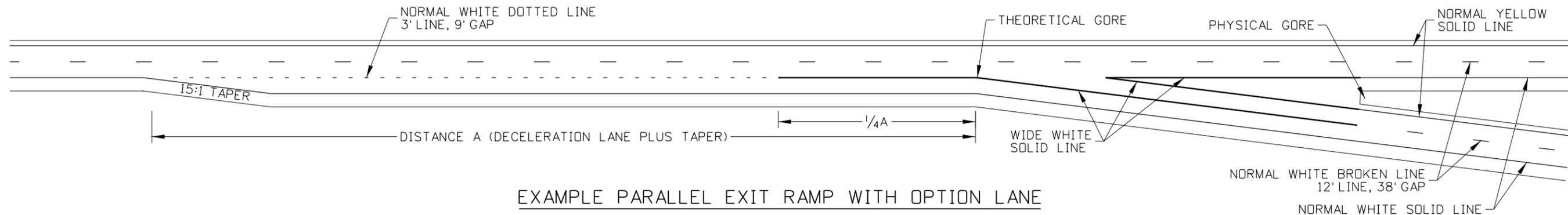
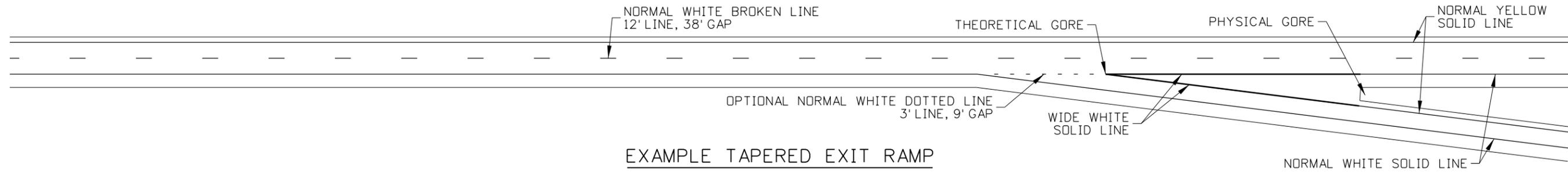
ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
PAVEMENT MARKINGS

English
 STANDARD DRAWING NO.
630-1
 SHEET 2 OF 4

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

PROFESSIONAL ENGINEER
 LICENSED
 13683
 RYAN D. LANCASTER
 STATE OF IDAHO



REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	03-20	RDL						

SCALES SHOWN
ARE FOR 11" X 17"
PRINTS ONLY

CADD FILE NAME:
630-1_0420.dgn

DRAWING DATE:
DECEMBER, 2016

**IDAHO
TRANSPORTATION
DEPARTMENT**



BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
PAVEMENT MARKINGS

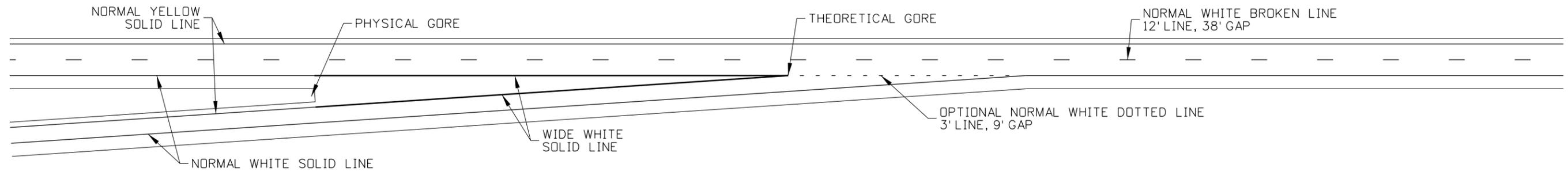
ORIGINAL STORED
AT: ITD,
Headquarters
3311 West State
Boise, Idaho

English

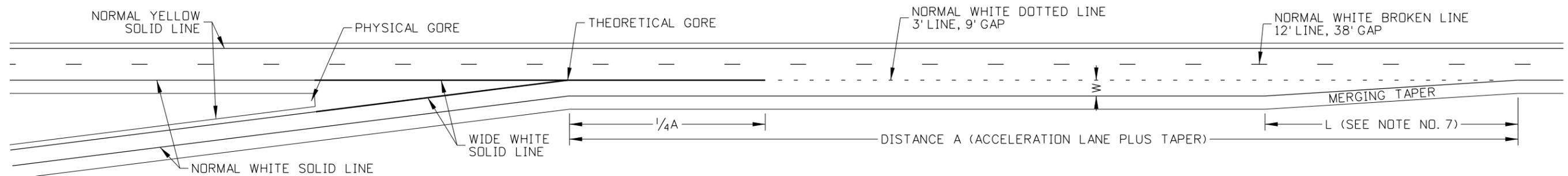
STANDARD DRAWING NO.
630-1

SHEET 3 OF 4

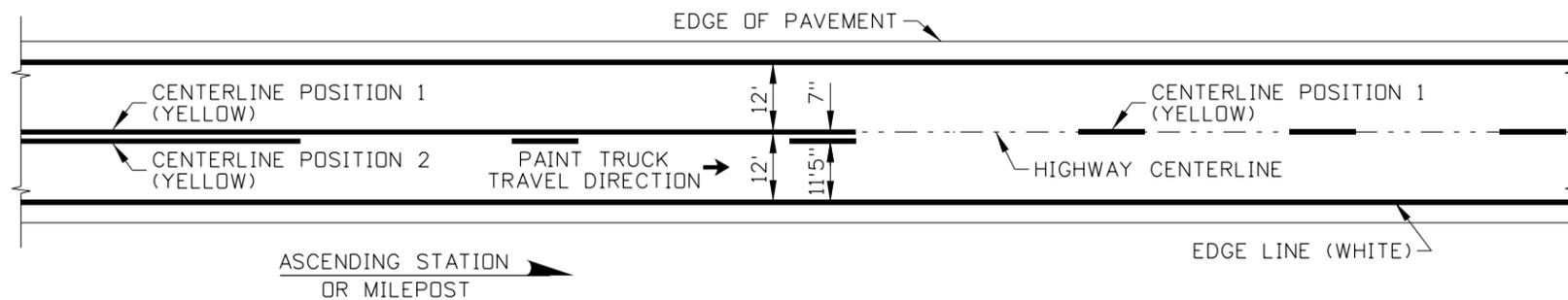
PROFESSIONAL ENGINEER
LICENSED
RYAN D. LANCASTER
13683
STATE OF IDAHO
MARCH 17, 2015



EXAMPLE TAPERED ENTRANCE RAMP

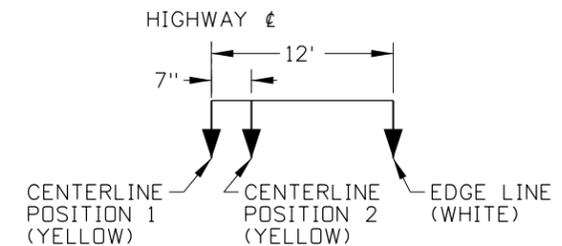


EXAMPLE PARALLEL ENTRANCE RAMP



PAVEMENT MARKINGS ON TWO-WAY HIGHWAYS

SEE NOTE NO. 10 AND PAINT TRUCK SETUP DETAIL



PAINT TRUCK SETUP DETAIL

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	03-20	RDL						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 630-1_0420.dgn
 DRAWING DATE: DECEMBER, 2016

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

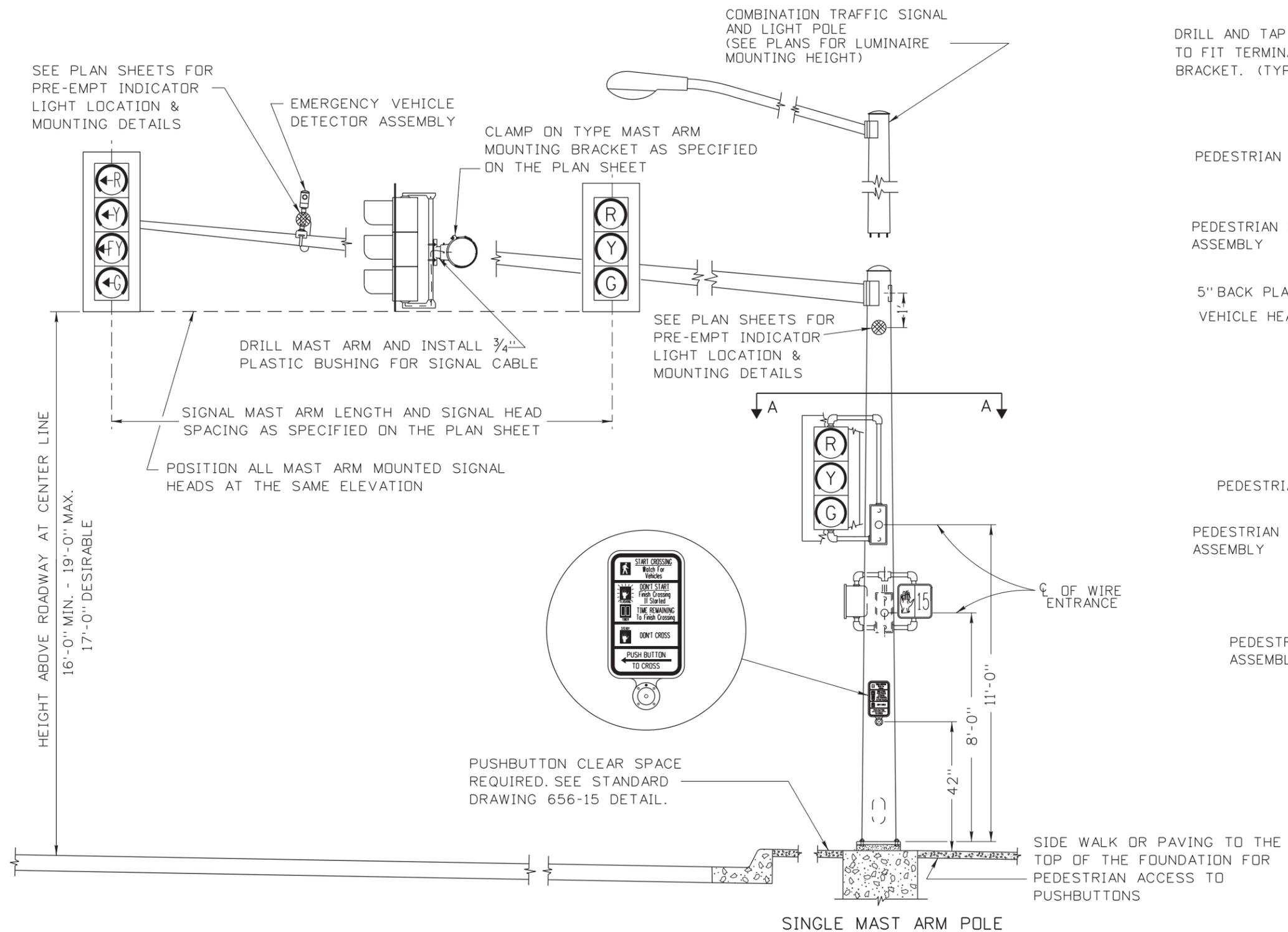
STANDARD DRAWING
PAVEMENT MARKINGS

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

STANDARD DRAWING NO. 630-1

SHEET 4 OF 4



NOTES:

1. THIS DRAWING SHOWS TYPICAL INSTALLATION DETAILS ONLY. SEE PLAN SHEETS FOR QUANTITY OF SIGNAL AND LIGHTING COMPONENTS TO BE INSTALLED.
2. ORIENTATION OF SIGNAL COMPONENTS SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLAN SHEETS.
3. SEE STANDARD DRAWING 656-3 FOR FOUNDATION DETAILS.
4. ALL SIGNAL COMPONENTS SHALL BE LEVELED AFTER THE POLE HAS BEEN PLUMBED.
5. SPECIFIC LOCATION OF EACH POLE INSTALLATION SHALL BE AS INDICATED ON THE PROJECT PLAN SHEETS.

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	12-94	HEB	6	04-14	HEB			
2	12-01	NQB						
3	07-05	HEB						
4	08-06	HEB						
5	07-10	HEB						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 656-1_0414.dgn

DRAWING DATE: AUGUST, 1994

IDAHO TRANSPORTATION DEPARTMENT



BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING

MAST ARM TRAFFIC SIGNAL POLES

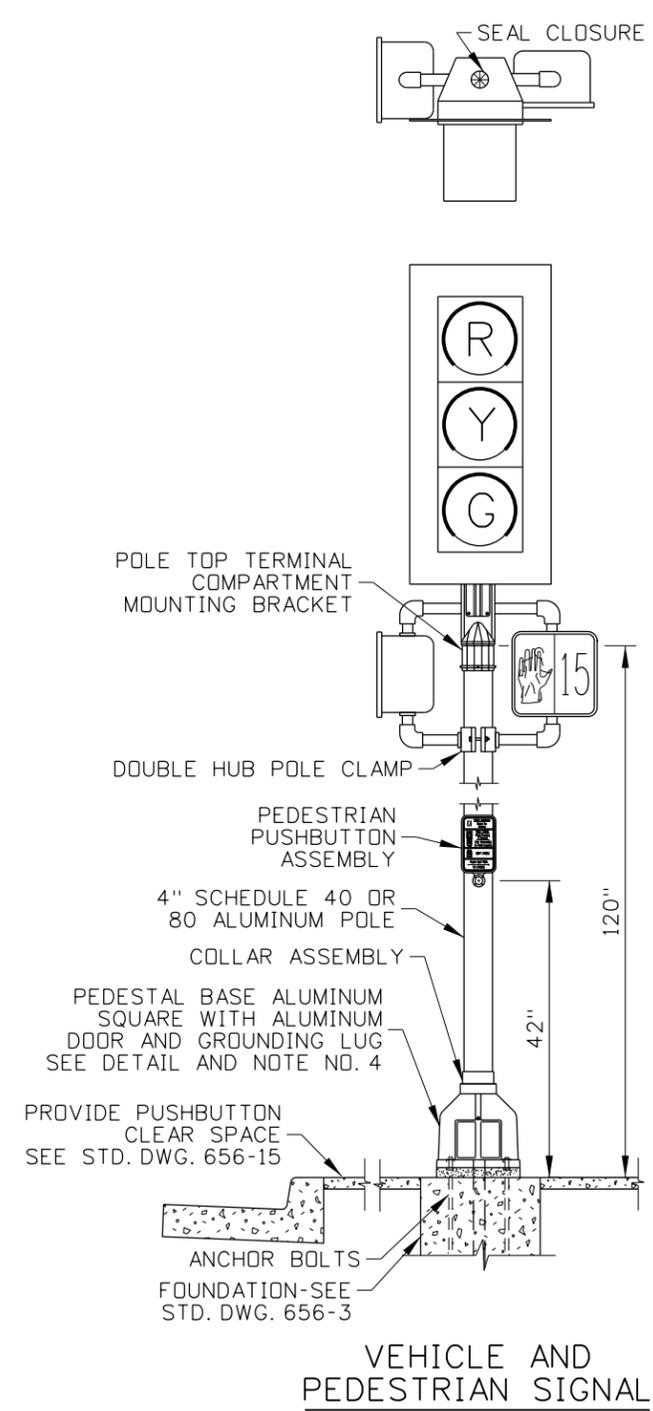
REQUIRES STD. DWG. 656-15

English

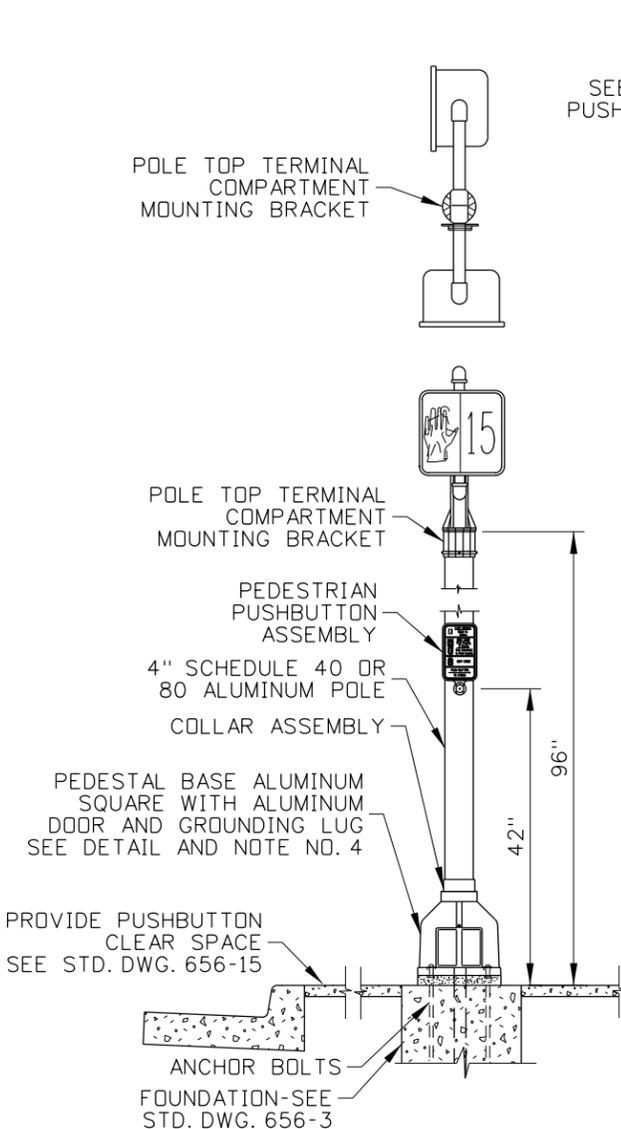
STANDARD DRAWING NO. **656-1**

SHEET 1 OF 1

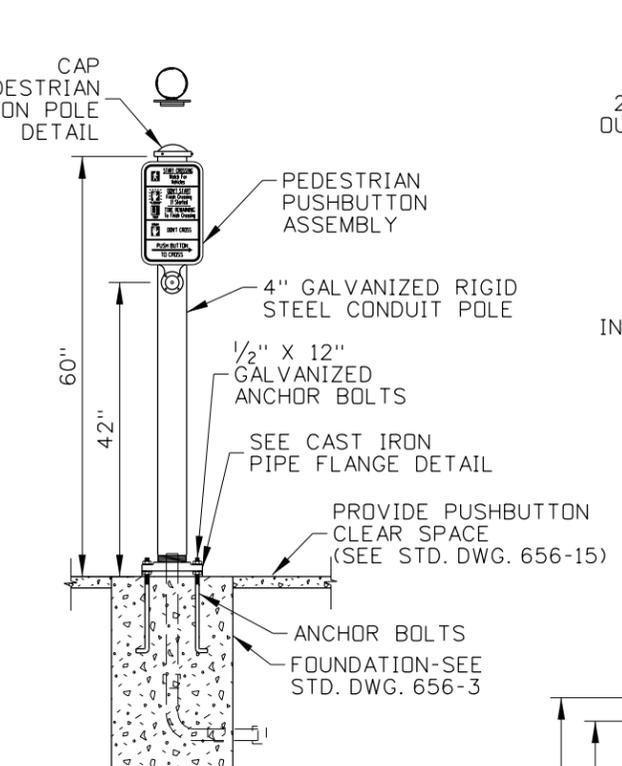
ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



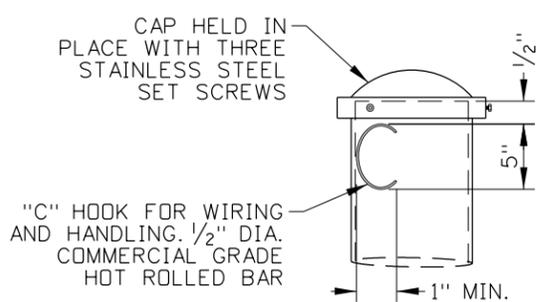
VEHICLE AND PEDESTRIAN SIGNAL



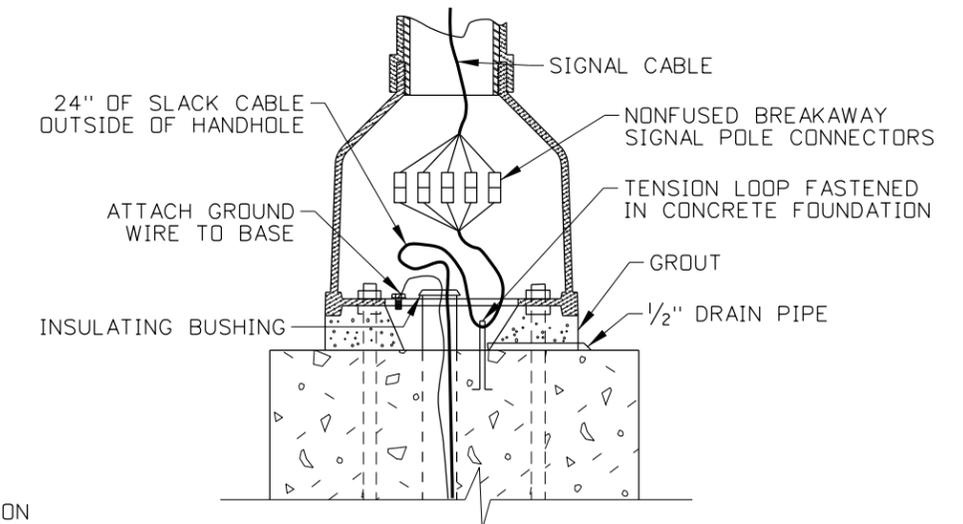
PEDESTRIAN SIGNAL



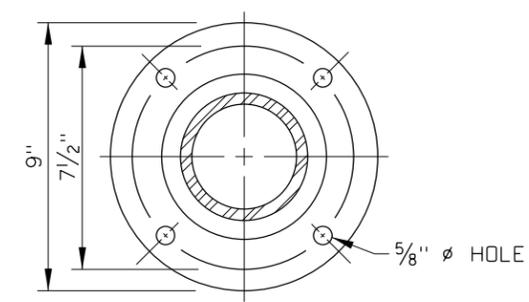
PEDESTRIAN PUSHBUTTON POLE



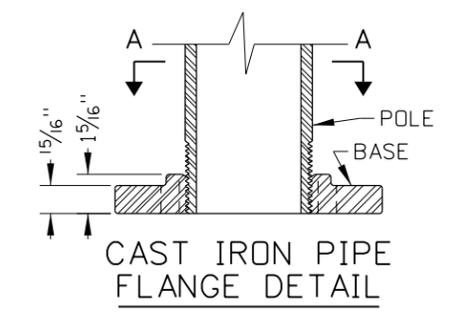
PEDESTRIAN PUSHBUTTON POLE DETAIL



FRANGIBLE ALUMINUM BASE DETAIL



SECTION A-A



CAST IRON PIPE FLANGE DETAIL

NOTES

1. THIS DRAWING SHOWS TYPICAL SIGNAL INSTALLATION DETAILS. THE DETAILS FOR COMPONENTS FROM DIFFERENT SUPPLIERS AND FOR QPL APPROVED PROPRIETARY SYSTEMS MAY VARY.
2. LOCATE POLES AS SHOWN IN THE PLANS AND IN ACCORDANCE WITH STANDARD DRAWING 656-15.
3. ORIENT SIGNAL COMPONENTS AS SHOWN UNLESS OTHERWISE SHOWN ON PLANS.
4. USE STEEL CONDUIT TO EXTEND CONDUIT BEYOND CONCRETE FOUNDATION AND STEEL CONDUIT ELBOWS IN THE FOUNDATION.
5. TERMINATE SPARE STUB OUTS WITH A STEEL COUPLING AND PLASTIC PUSH PLUG AT BOTH ENDS.
6. A FRANGIBLE COUPLING SYSTEM BASE CAN BE SUBSTITUTED FOR A FRANGIBLE CAST ALUMINUM BASE.
7. LEVEL SIGNAL COMPONENTS AFTER THE POLE HAS BEEN PLUMBED.
8. DRAWING NOT TO SCALE.

REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE
1	12-94	HEB	6	04-14	HEB		
2	08-06	NQB	7	12-16	HEB		
3	12-07	HEB	8	03-20	RDL		
4	07-09	HEB	9	02-22	RDL		
5	07-10	HEB					

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 656-2_0422.dgn
 DRAWING DATE: AUGUST, 1994

IDAHO TRANSPORTATION DEPARTMENT

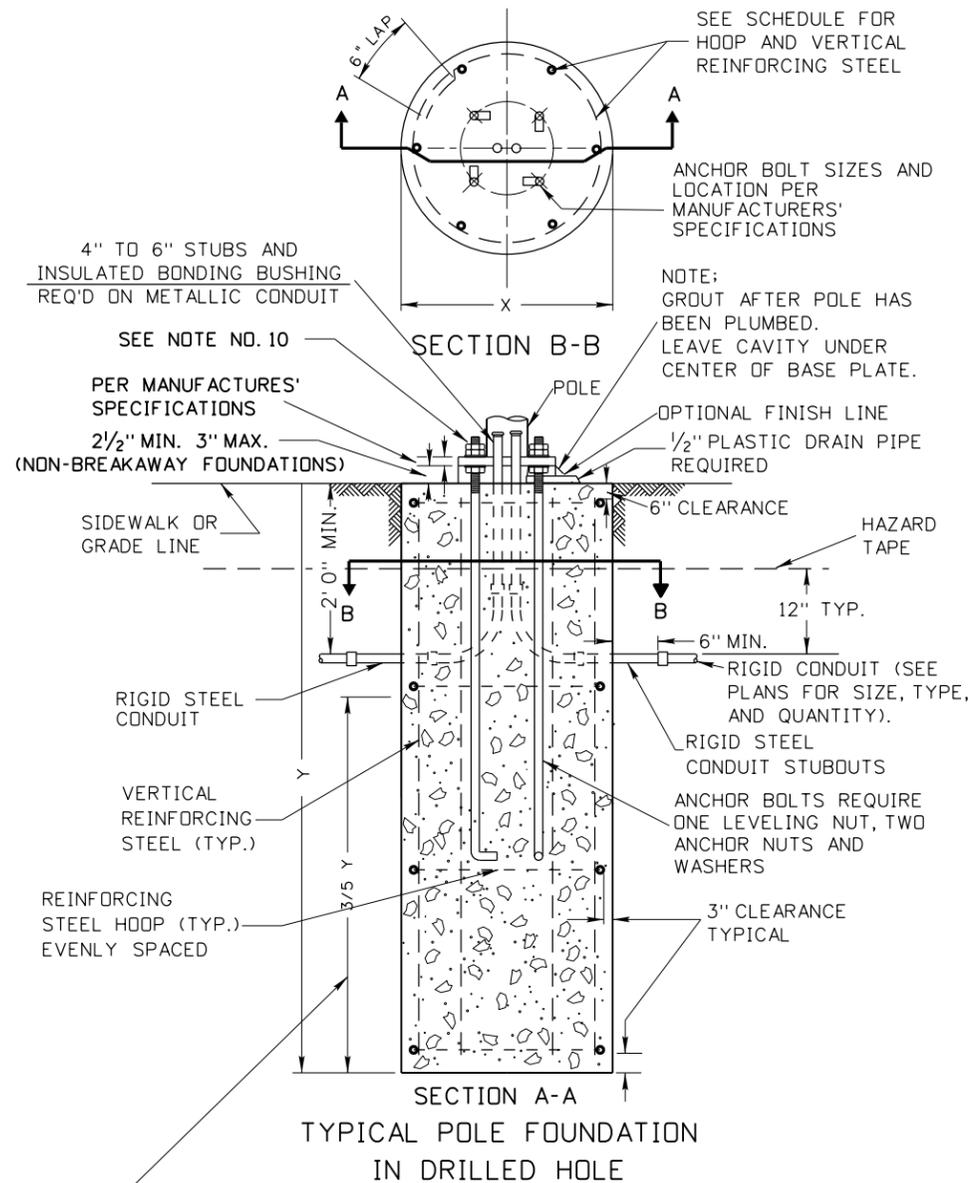
BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
FRANGIBLE BASE TRAFFIC SIGNAL POLES
 REQUIRES STD. DWG. 656-3 AND 656-15

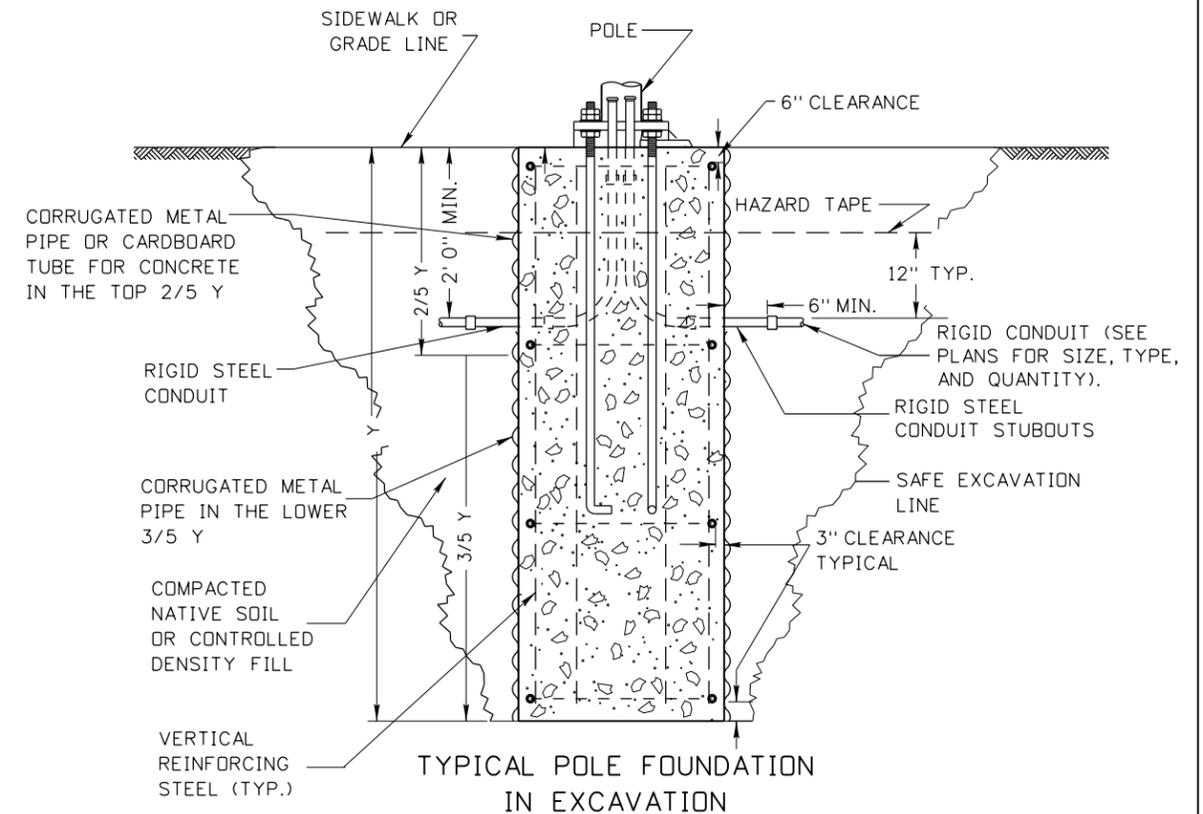
English
 STANDARD DRAWING NO.
656-2
 SHEET 1 OF 1

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



GENERAL NOTES:

1. THE FOUNDATIONS SHALL BE LOCATED AS INDICATED ON THE PROJECT PLAN SHEETS.
2. FOUNDATION REINFORCING STEEL CAGES MAY BE WELDED IF THE REINFORCING STEEL CONFORMS TO AASHTO M 31 AND ALL WELDING CONFORMS TO ANSI/AWS D1.4 (STRUCTURAL WELDING CODE - REINFORCING STEEL).
3. REINFORCING STEEL IN POLE FOUNDATIONS SHALL BE 60 KSI STEEL.
4. STEEL TEMPLATE REQUIRED FOR ANCHOR BOLT PLACEMENT.
5. SPARE STUBOUTS WHEN SHOWN ON PLAN SHEETS SHALL BE TERMINATED WITH A STEEL COUPLING AND FOAM PLUG AT BOTH ENDS.
6. CLASS 40A CONCRETE SHALL BE USED IN POLE FOUNDATIONS.
7. FOUNDATION CONCRETE SHALL ACHIEVE 100% STRENGTH AND CURE FOR A MINIMUM OF 7 DAYS BEFORE ANY LOADING IS APPLIED.
8. FILLER JOINT MATERIAL WILL BE PLACED AROUND POLE FOUNDATION WHEN POLE FOUNDATION IS IN CONTACT WITH SIDEWALK.
9. ELEVATION OF TOP OF POLE FOUNDATION SHALL MATCH THE ADJACENT PAVEMENT EDGE OR SIDEWALK ELEVATION.
10. ANCHOR BASE ASSEMBLIES SHALL BE INSTALLED AND TIGHTENED IN ACCORDANCE WITH SUBSECTION 619.03 OF THE ITD STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND THE SUPPLEMENTAL SPECIFICATIONS.
11. DRAWING NOT TO SCALE.



EXCAVATION NOTES:

1. IF CORRUGATED METAL PIPE IS USED UP TO SIDEWALK OR GRADE LINE, CUT OUT HOLE FOR THE CONDUITS WILL BE EQUAL TO THE DIAMETER OF CONDUIT OR CONDUITS PLUS ONE INCH.
2. WHEN NATIVE SOIL IS USED FOR BACKFILL, IT SHALL BE COMPACTED IN ACCORDANCE WITH SUBSECTION 210.03 OF THE IDAHO STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND SUPPLEMENTAL SPECIFICATIONS.
3. IF CONTROL DENSITY FILL IS USED FOR BACK FILL, IT SHALL HAVE A COMPRESSIVE STRENGTH OF 100 PSI TO 300 PSI.
4. DRAWING NOT TO SCALE.

THE LOWER 3/5 OF THE FOUNDATION TO BE PLACED AGAINST UNDISTURBED SOIL UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE UPPER 2/5 OF THE FOUNDATION MAY BE FORMED AS NEEDED.

POLE FOUNDATION SCHEDULE												
POLE TYPE	MT.HT.	MASTARM LENGTH	FOUNDATION TYPE	X	Y	REINFORCING STEEL HOOPS			VERTICAL REINFORCING STEEL		CUBIC YARDS CONCRETE	
						QTY.	SIZE	LIN.FT.	QTY.	SIZE		LIN.FT.
PEDESTRIAN SIGNAL POLE	-	-	A	2'-0"	5'-0"	4	#4	20'-10"	6	#4	25'-6"	.6
SIGNAL POLE	-	20' - 45'	D	3'-0"	9'-0"	5	#4	41'-10"	8	#6	66'-0"	2.4
PEDESTRIAN PUSHBUTTON POLE	4'-0"	-	E	1'-6"	2'-6"	-	-	-	-	-	-	0.2
DUAL MAST ARM SIGNAL POLE	-	ALL	F	3'-0"	12'-0"	8	#5	66'-10"	12	#6	135'-0"	3.1
SIGNAL POLE	-	50' - 55'	F	3'-0"	12'-0"	8	#5	66'-10"	12	#6	135'-0"	3.1

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 656-3_0517.dgn
 DRAWING DATE: MAY, 2017

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: KEVIN SABLAN
 DESIGN/TRAFFIC SERVICES ENGINEER

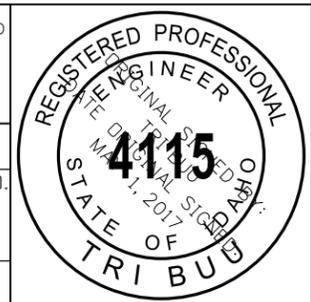
STANDARD DRAWING
MASTARM SIGNAL POLE AND PEDESTRIAN POLE FOUNDATION DETAILS

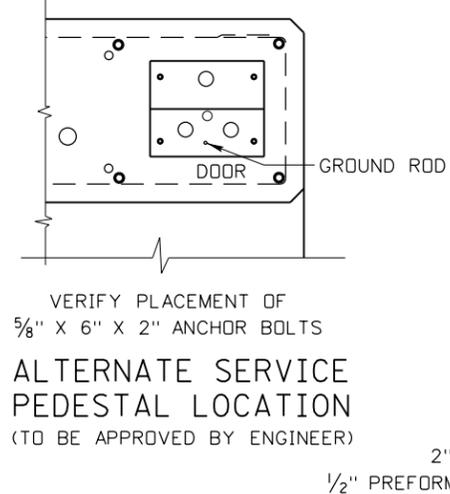
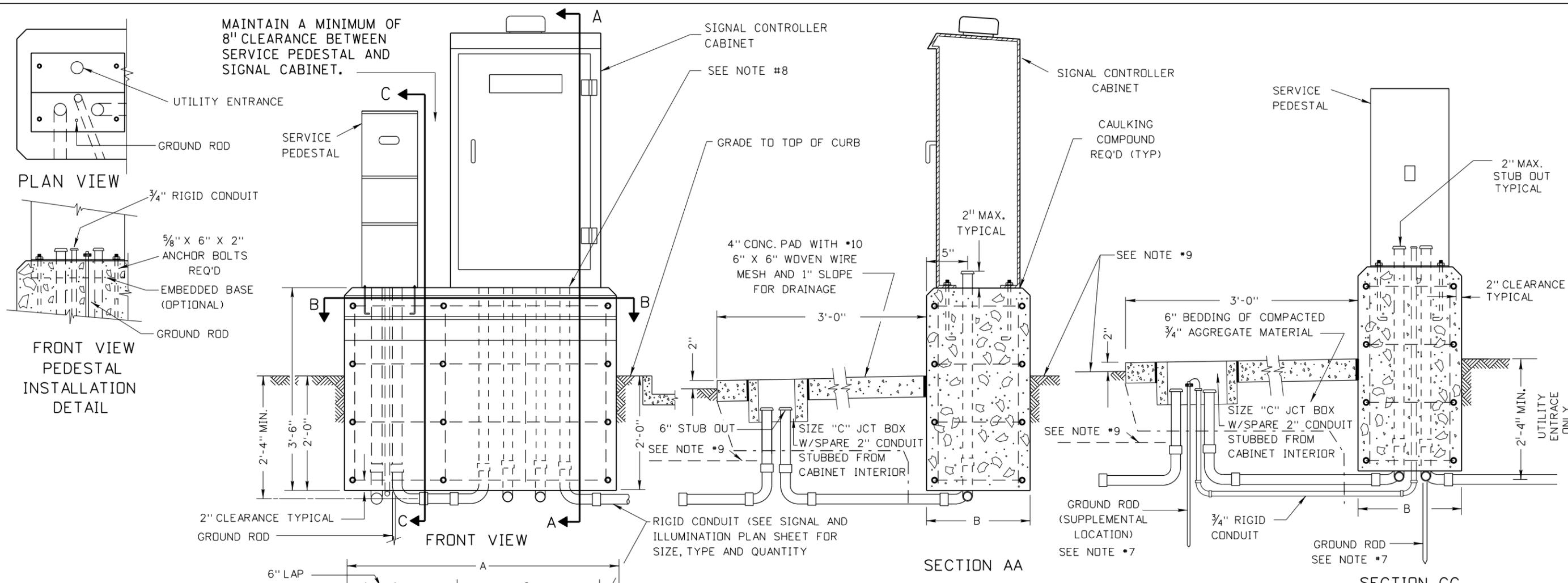
ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

English

STANDARD DRAWING NO. **656-3**

SHEET 1 OF 2





FOUNDATION SCHEDULE
CABINET AND SERVICE PEDESTAL

CABINET TYPE	FOUNDATION TYPE	A	B	C	D	E	HOOPS			VERTICAL RODS			CU. YDS. CONC. FOUNDATION PAD	CABINET ANCHOR BOLTS		SERVICE PEDESTAL ANCHOR BOLTS		
							NO.	SIZE	LIN. FT.	NO.	SIZE	LIN. FT.		NO.	SIZE	QNTY.	NO.	SIZE
SIGNAL	M	5'-6"	1'-8"	-	-	1'-0"	4	#4	54'-0"	8	#4	25'-4"	1.2	.2	2	3/4" X 18" X 2 1/4"	4	5/8" X 6" X 2"
	P & R	6'-8"	2'-5"	3'-4 3/4"	1'-6 1/2"	-	4	#4	69'-4"	8	#4	25'-4"	2.1	.25	4	3/4" X 18" X 2 1/4"	4	5/8" X 6" X 2"

- NOTES:
1. THE FOUNDATION SHALL BE LOCATED AS INDICATED ON THE PROJECT PLAN SHEETS.
 2. FOUNDATION REBAR CAGES MAY BE WELDED IF THE REBAR CONFORMS TO ASTM A706/A706M AND ALL WELDING CONFORMS TO ANSI/AWS D1.4 (STRUCTURAL WELDING CODE - REINFORCING STEEL).
 3. ALL CONDUITS, ELBOWS, AND COUPLINGS WITHIN AND PROTRUDING FROM THE FOUNDATION SHALL BE RIGID STEEL. THE REMAINING CONDUITS SHALL BE AS SHOWN ON THE PLANS.
 4. STUBOUTS SHALL BE TERMINATED WITH A STEEL BONDING BUSHING.
 5. FOAM PLUG REQUIRED AT BOTH ENDS OF SPARE CONDUITS.
 6. GROUND IN ACCORDANCE WITH N.E.C.
 7. CONDUIT SHALL BE INSTALLED IN SUCH A MANNER AS TO NOT CAUSE MODIFICATION OF THE CABINETS.
 8. GRADE AREA TO PROVIDE DRAINAGE AWAY FROM CABINET FOUNDATION.
 9. 6" WIDE ELECTRICAL HAZARD TAPE INSTALLED 1'-0" ABOVE CONDUIT (TYPICAL OF ALL CONDUIT PLACED IN GROUND).
 10. IF SUPPLIED, USE SERVICE PEDESTAL BASE FOR ANCHOR BASE TEMPLATE.
 11. NOT TO SCALE

REVISIONS

NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY
1	08-96	NQB						
2	12-04	HEB						
3	05-05	HEB						
4	05-14	HEB						

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 656-5_0614.dgn

DRAWING DATE: DECEMBER, 1994

IDAHO TRANSPORTATION DEPARTMENT

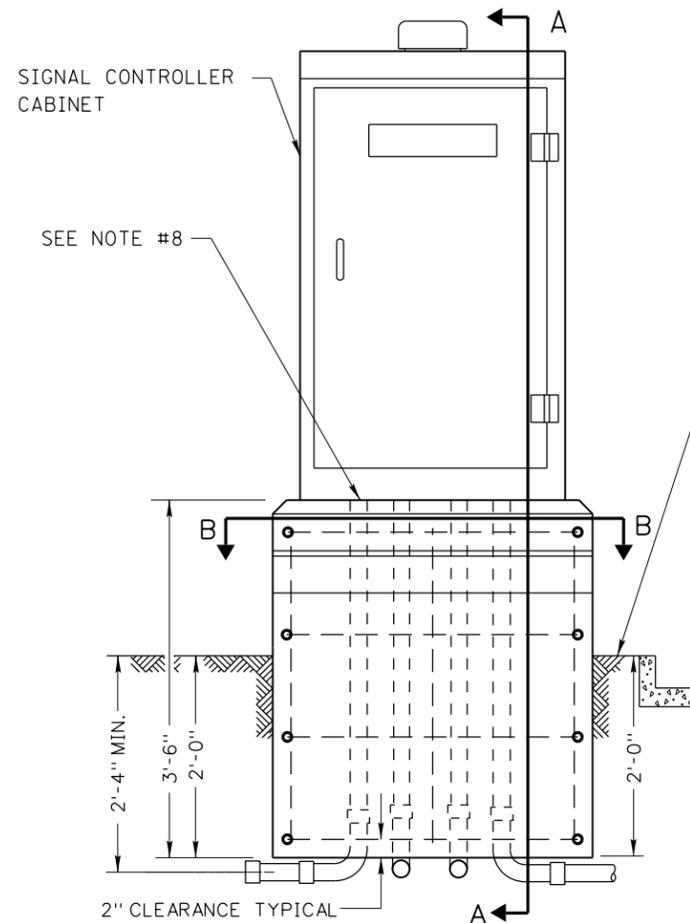
BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN
DESIGN/TRAFFIC SERVICES ENGINEER

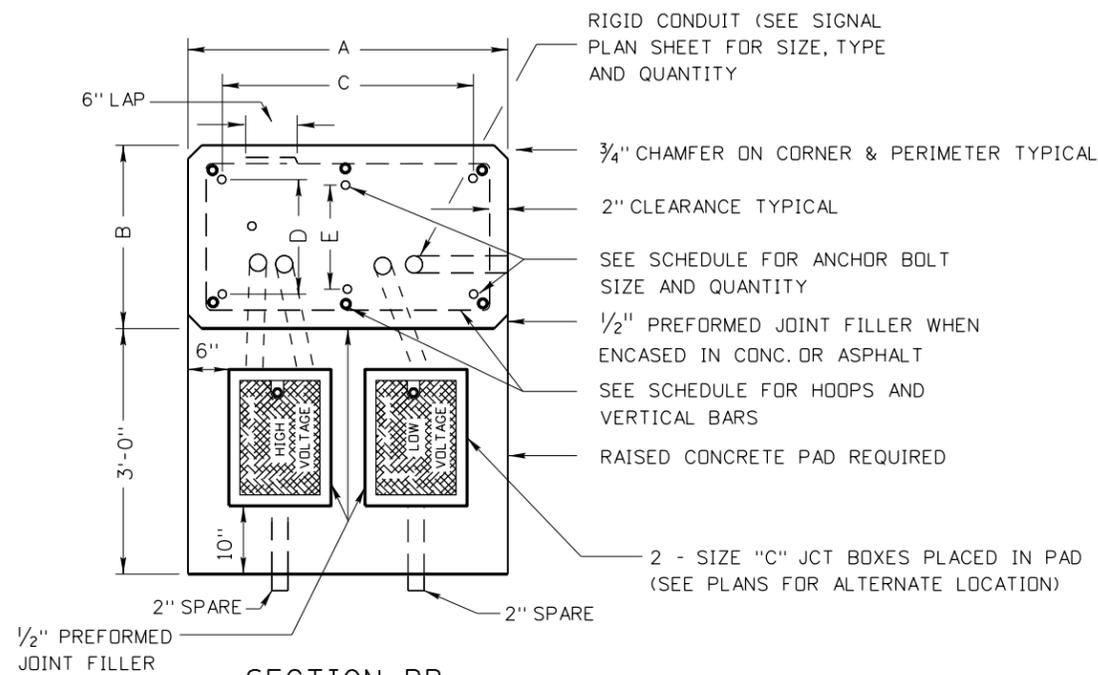
STANDARD DRAWING
SIGNAL CABINET & SERVICE PEDESTAL FOUNDATION DETAILS

English
STANDARD DRAWING NO. 656-5
SHEET 1 OF 1

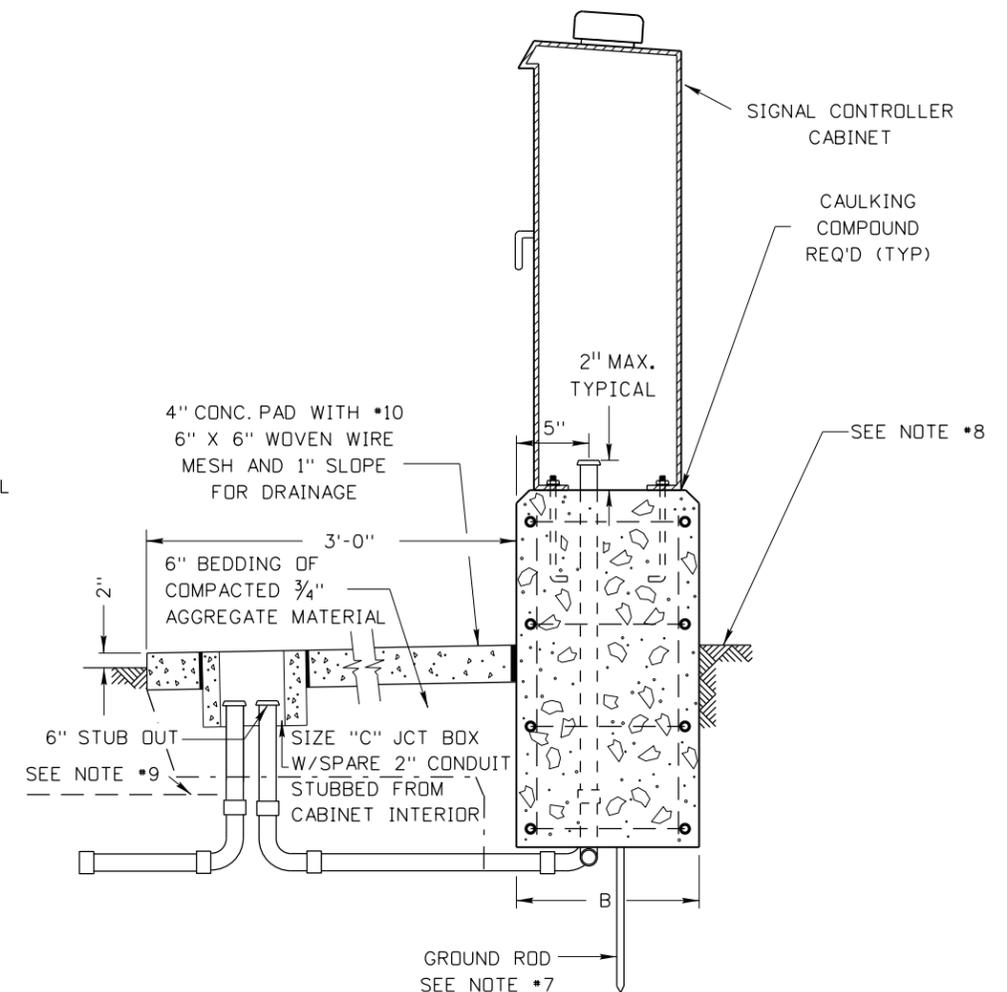
ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



FRONT VIEW



SECTION BB



SECTION AA

NOTES:

1. THE FOUNDATION SHALL BE LOCATED AS INDICATED ON THE PROJECT PLAN SHEETS.
2. FOUNDATION REBAR CAGES MAY BE WELDED IF THE REBAR CONFORMS TO ASTM A706/A706M AND ALL WELDING CONFORMS TO ANSI/AWS D1.4 (STRUCTURAL WELDING CODE - REINFORCING STEEL).
3. ALL CONDUITS, ELBOWS, AND COUPLINGS WITHIN AND PROTRUDING FROM THE FOUNDATION SHALL BE RIGID STEEL. THE REMAINING CONDUITS SHALL BE AS SHOWN ON THE PLANS.
4. STUBOUTS SHALL BE TERMINATED WITH A STEEL BONDING BUSHING.
5. FOAM PLUG REQUIRED AT BOTH ENDS OF SPARE CONDUITS.
6. GROUND IN ACCORDANCE WITH N.E.C.
7. CONDUIT SHALL BE INSTALLED IN SUCH A MANNER AS TO NOT CAUSE MODIFICATION OF THE CABINETS.
8. GRADE AREA TO PROVIDE DRAINAGE AWAY FROM CABINET FOUNDATION.
9. 6" WIDE ELECTRICAL HAZARD TAPE INSTALLED 1'-0" ABOVE CONDUIT (TYPICAL OF ALL CONDUIT PLACED IN GROUND).
10. NOT TO SCALE.

CABINET FOUNDATION SCHEDULE

CABINET TYPE	FOUNDATION TYPE	A	B	C	D	E	HOOPS			VERTICAL RODS			CU. YDS. CONC.		CABINET ANCHOR BOLT	
							NO.	SIZE	LIN. FT.	NO.	SIZE	LIN. FT.	FOUNDATION	PAD	QNTY.	SIZE
SIGNAL	M	2'-9"	1'-8"	-	-	1'-0"	4	#4	32'-0"	6	#4	19'-0"	.6	.14	2	3/4" X 18" X 2 1/4"
	P & R	3'-11"	2'-5"	3'-4 3/4"	1'-6 1/2"	-	4	#4	47'-4"	6	#4	19'-0"	1.23	.14	4	3/4" X 18" X 2 1/4"

REVISIONS								
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE	BY

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY
 CADD FILE NAME: 656-6_0614.dgn
 DRAWING DATE: MAY, 2014

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN
 DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING
SIGNAL CABINET FOUNDATION DETAIL

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

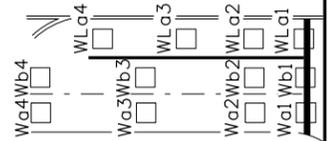
English
 STANDARD DRAWING NO.
656-6
 SHEET 1 OF 1

POSITION

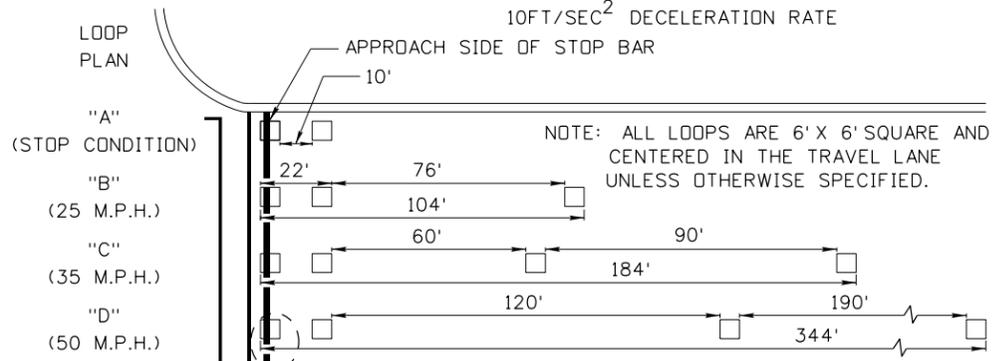
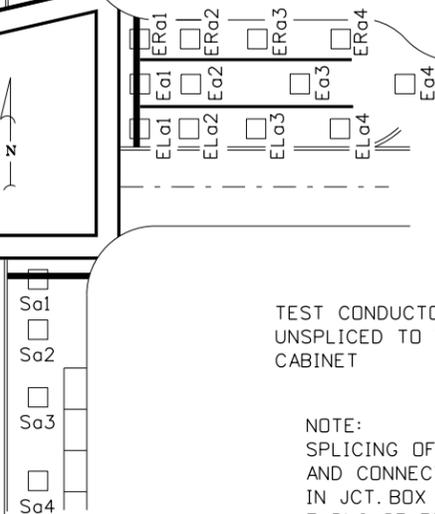
- MAGNETIC ORIENTATION OF APPROACHING TRAVEL LANE.
N = NORTH, S = SOUTH, ETC.
- VEHICULAR MOVEMENT IN TRAVEL LANE: L = LEFT TURN; R = RIGHT TURN; NO ENTRY = THROUGH
- LANE DESIGNATION FOR EACH MOVEMENT: a-z IN ASCENDING ORDER, a BEING NEAREST THE CURB OR EDGE OF PAVEMENT.

- Na4
- Na3
- Na2
- Na1

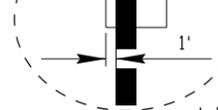
- LOOP NUMBER IN EACH TRAVEL LANE: 1 - 4 IN ASCENDING ORDER, 1 BEING THE LOOP NEAREST THE STOP BAR.
- LOOP NUMBERING PROCEDURE EXAMPLE
- | | | | |
|---|---|---|---|
| W | L | a | 3 |
| 1 | 2 | 3 | 4 |



LOOP NUMBERING SYSTEM



DETECTOR LOOP SPACING PLAN



THE 1' DIMENSION SHALL BE USED WITH/WITHOUT A CROSSWALK.

TEST CONDUCTOR: UNSPLICED TO THE CABINET

NOTE: SPLICING OF MULTIPLE LOOP CONDUCTORS AND CONNECTION TO THE LEAD-IN ALLOWED IN JCT. BOX ONLY. ALL LOOPS REQUIRE TWO TURNS OF FOUR CONDUCTOR CABLE.

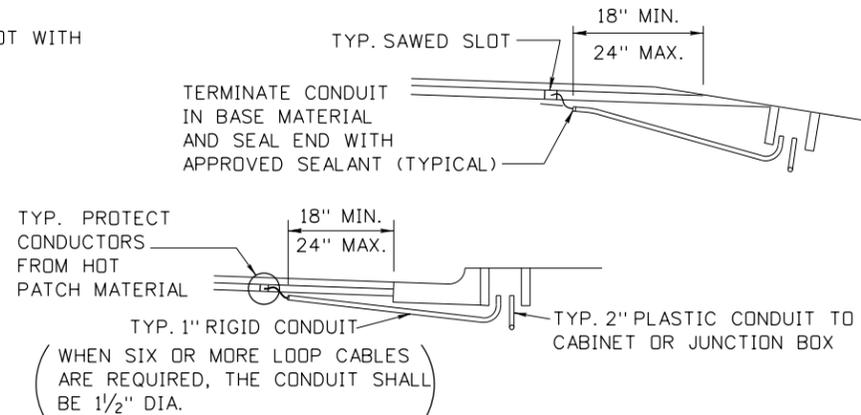
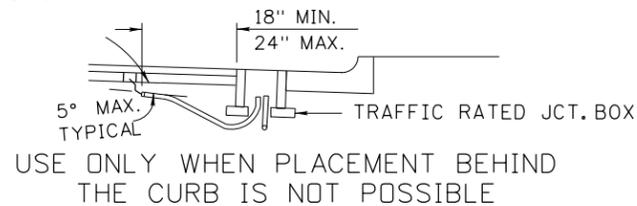
THE LOOPS IN EACH LANE SHALL BE WOUND IN THE SAME DIRECTION.

DUAL PURPOSE LOOP SHALL BE LAST LOOP IN SYSTEM

LOOP SYSTEM AND JUNCTION BOX WIRING DIAGRAM

LOOP CONDUCTOR INSTALLATION

- NOTES:
- METHOD "C" MAY BE USED ONLY WITH PRE-FORMED LOOPS.
 - HYDRO CLEAN AND AIR DRY SLOTS AFTER CUTTING AND PRIOR TO CABLE INSTALLATION.
 - BED LOOP CONDUCTORS IN SEALANT FOR TOTAL ENCAPSULATION.
 - SEAT LOOP CONDUCTORS IN SLOT WITH A BLUNT INSTRUMENT.



JUNCTION BOX AND CONDUIT LOCATION

NOTES:

- ALL CONDUCTOR SPLICES SHALL BE SOLDERED AND WATERPROOFED WITH AN APPROVED SPLICE KIT.
- THE FOIL SHIELD SHALL BE INSULATED TO PREVENT GROUNDING AT THE JUNCTION BOX.
- * SPLICE DUAL PURPOSE LOOPS TO THE RED AND GREEN CONDUCTORS.

*10 A.W.G. TEST CONDUCTOR REQUIRED

COLOR CODED LOOP IDENTIFICATION SYSTEM TAPE

TOP OF JUNCTION BOX

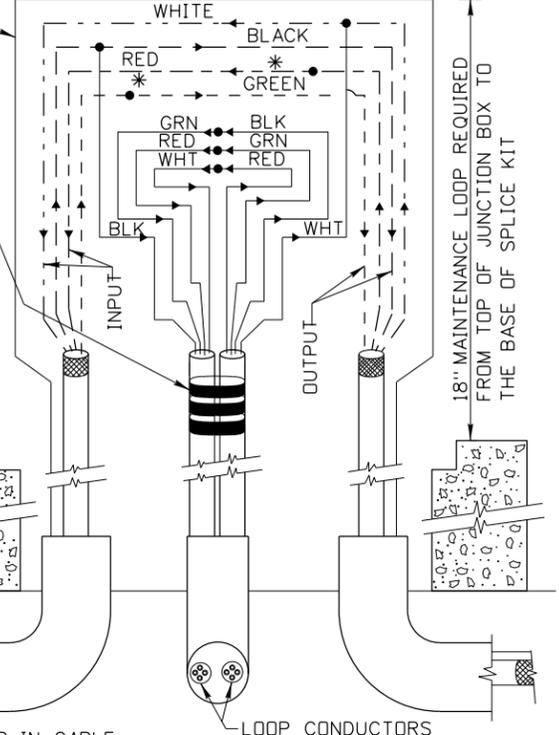
*10 A.W.G. TEST CONDUCTOR

4 CONDUCTOR LEAD IN CABLE: INSULATE AND SEAL END OF EACH UNUSED CONDUCTOR IN LAST JUNCTION BOX.

DO NOT GROUND FOIL SHIELD

DIRECTION OF TRAVEL

LOOP SPLICE DETAIL AT JUNCTION BOX



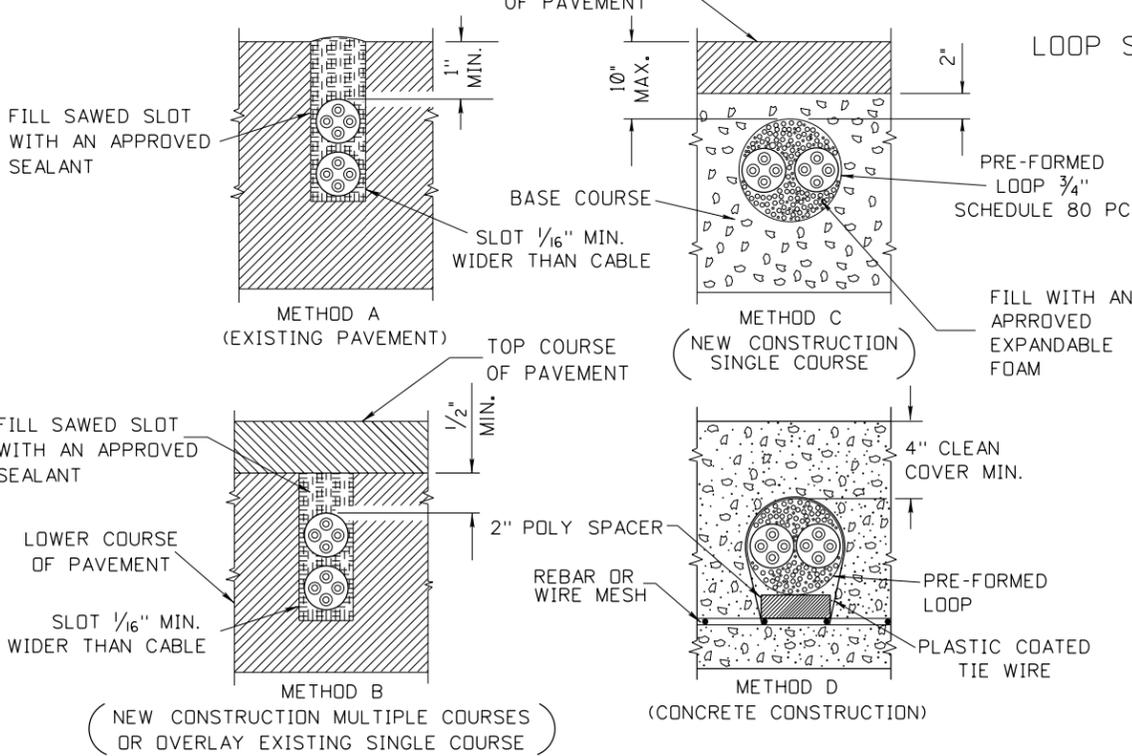
LANE NO.	TAPE COLOR
1	BLACK
2	WHITE
3	RED
4	GREEN
5	ORANGE
6	BLUE

IN ASCENDING ORDER - BLACK SHALL BE USED FOR THE LANE NEAREST THE CURB OR EDGE OF PAVEMENT.

EXAMPLE: CONDUCTORS FOR LOOP 1 LANE 1 REQUIRE 1 BAND OF BLACK TAPE.

CONDUCTORS FOR LOOP 3 LANE 4 REQUIRE 3 BANDS OF GREEN TAPE.

COLOR CODED LOOP IDENTIFICATION SYSTEM



REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE
1	12-93	HEB	6	08-08	NQB		
2	12-94	HEB	7	07-10	HEB		
3	03-96	HEB					
4	07-03	HEB					
5	08-06	HEB					

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 656-10_0710.dgn

DRAWING DATE: DECEMBER, 1994

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO



ORIGINAL SIGNED BY: LOREN THOMAS
ASSISTANT CHIEF ENGINEER (DEVELOPMENT)

ORIGINAL SIGNED BY: TOM COLE
CHIEF ENGINEER

STANDARD DRAWING

LOOP DETECTORS

10FT/SEC² DECELERATION RATE

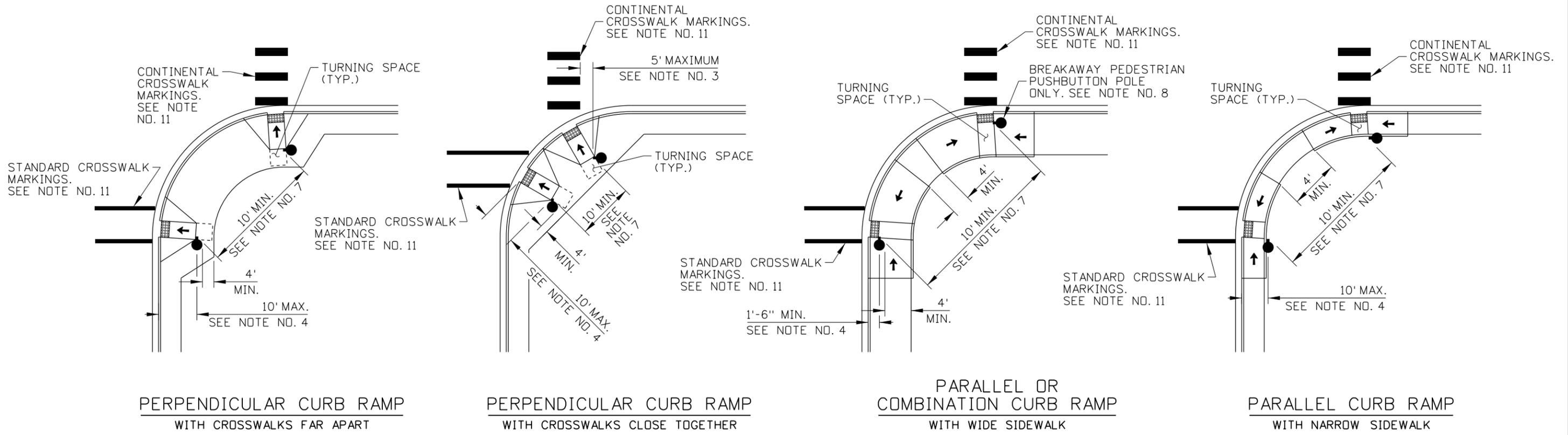
English

STANDARD DRAWING NO. 656-10

SHEET 1 OF 1

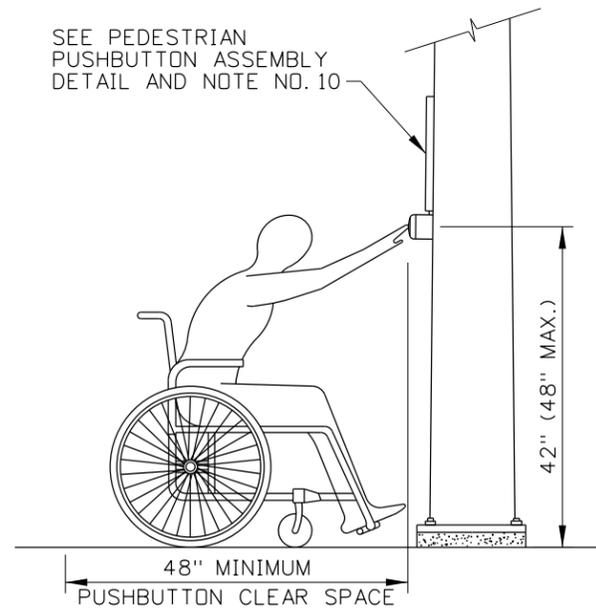
ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho



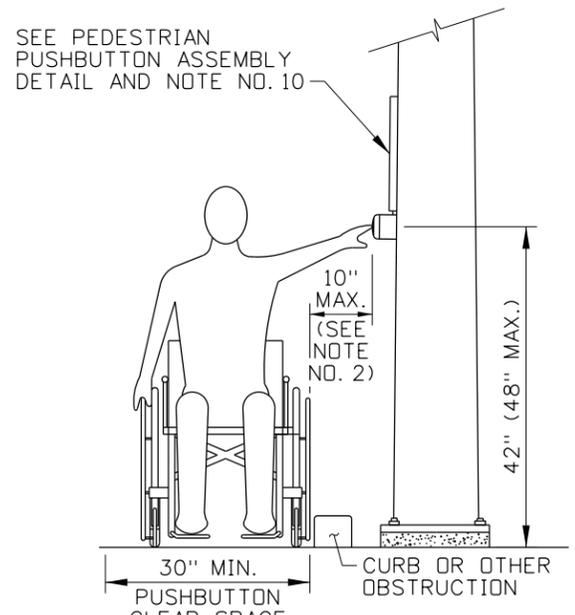


NOTES

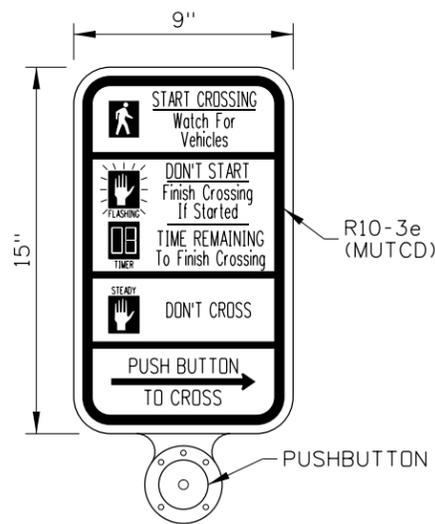
- FOUR TYPICAL CONFIGURATIONS ARE SHOWN. FOR OTHER CONFIGURATIONS, THE MAXIMUM AND MINIMUM DIMENSIONS SHOWN REMAIN APPLICABLE. WHERE CURB AND CURB RAMPS ARE NOT PRESENT, MEASURE FROM THE EDGE OF TRAVELED WAY.
- PROVIDE A PUSHBUTTON CLEAR SPACE, WITH 30" BY 48" MINIMUM DIMENSIONS, ADJACENT TO PEDESTRIAN PUSHBUTTONS. ENSURE THAT THE CLEAR SPACE SURFACE IS FIRM, STABLE, AND SLIP RESISTANT. POSITION THE CLEAR SPACE SO THE PUSHBUTTON CAN BE ACCESSED WITH EITHER A FORWARD OR PARALLEL APPROACH. AN OBSTRUCTION, UP TO 10" IN DEPTH, BETWEEN THE CLEAR SPACE AND THE PUSHBUTTON IS PERMITTED FOR A PARALLEL APPROACH. WHERE CURB RAMPS ARE USED, THE TURNING SPACE FOR THE CURB RAMP MAY DOUBLE AS THE CLEAR SPACE.
- PLACE PUSHBUTTON BETWEEN THE EDGE OF THE CROSSWALK LINE (EXTENDED) FARTHEST FROM THE CENTER OF THE INTERSECTION AND THE SIDE OF A CURB RAMP (IF PRESENT), BUT NOT MORE THAN 5' FROM THE SAID CROSSWALK LINE.
- PLACE PUSHBUTTON BEHIND THE FACE OF CURB OR OUTSIDE THE EDGE OF TRAVELED WAY A MINIMUM DISTANCE OF 1'-6" AND A MAXIMUM DISTANCE OF 10'.
- ENSURE THAT THE FACE OF THE PUSHBUTTON IS PARALLEL TO THE CROSSWALK TO BE USED.
- MOUNT THE PUSHBUTTON 42" ABOVE THE CLEAR SPACE. THE MAXIMUM MOUNTING HEIGHT IS 48".
- PROVIDE A MINIMUM DISTANCE OF 10' BETWEEN PEDESTRIAN PUSHBUTTONS. WHERE THERE ARE PHYSICAL CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10' SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.
- PUSHBUTTONS MAY BE INSTALLED ON A SIGNAL POLE OR PEDESTRIAN PUSHBUTTON POLES. ENSURE THAT POLES WITHIN 10' OF THE FACE OF CURB OR EDGE OF TRAVELED WAY HAVE BREAKAWAY FEATURES.
- INSTALL ACCESSIBLE PEDESTRIAN SIGNALS WHEN RECOMMENDED BY AN ENGINEERING STUDY.
- USE MODULAR OR CAST PEDESTRIAN PUSHBUTTON ASSEMBLIES. DO NOT USE H FRAME PEDESTRIAN PUSHBUTTON ASSEMBLIES. USE THE R10-3e(MUTCD) SIGN ON THE PUSHBUTTON ASSEMBLY.
- CROSSWALK MARKINGS MAY VARY.
- DRAWINGS NOT TO SCALE.



FORWARD APPROACH
SEE NOTE NOS. 2 & 6



PARALLEL APPROACH
SEE NOTE NOS. 2 & 6



PEDESTRIAN PUSHBUTTON ASSEMBLY DETAIL
SEE NOTE NO. 10

REVISIONS							
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE

SCALES SHOWN ARE FOR 11" X 17" PRINTS ONLY

CADD FILE NAME: 656-15_0614.dgn

DRAWING DATE: MAY, 2014

IDAHO TRANSPORTATION DEPARTMENT

BOISE IDAHO

ORIGINAL SIGNED BY: CARL D. MAIN
DESIGN/TRAFFIC SERVICES ENGINEER

STANDARD DRAWING

PEDESTRIAN PUSHBUTTON PLACEMENT

English

STANDARD DRAWING NO. 656-15

SHEET 1 OF 1

ORIGINAL STORED AT: ITD, Headquarters 3311 West State Boise, Idaho

PROFESSIONAL ENGINEER

RYAN D. LANCASTER

13683

JUN 17 2014